

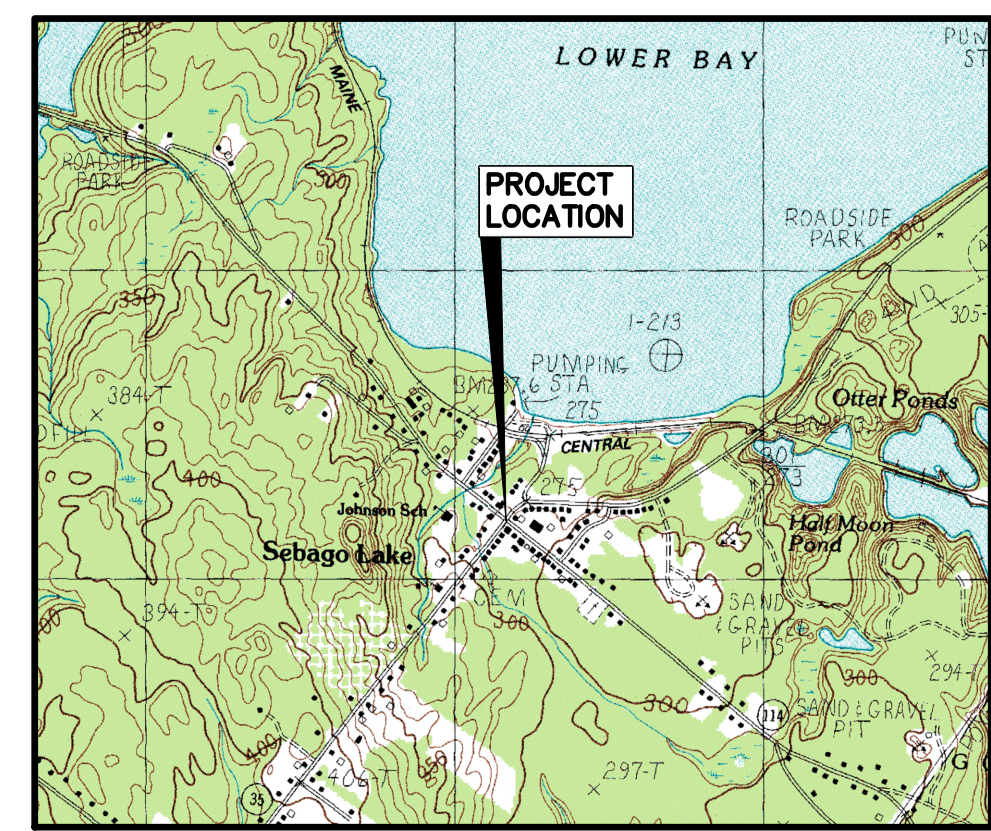
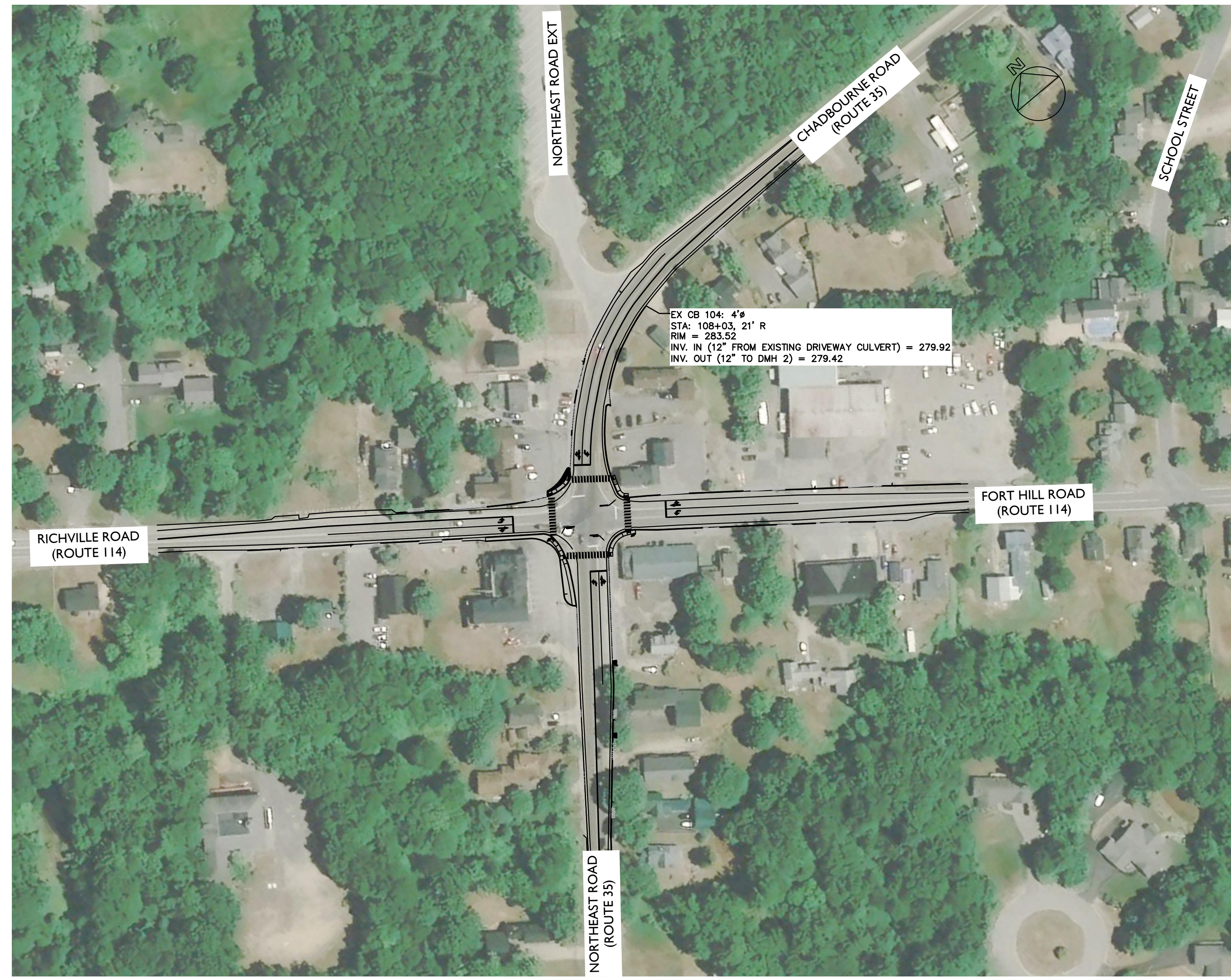
# ROUTE 114 & 35 INTERSECTION LAYOUT & SIGNALIZATION

STANDISH, MAINE  
CUMBERLAND COUNTY

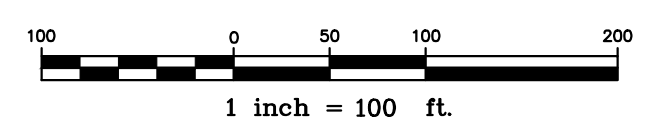
NOVEMBER 2023

CONVENTIONAL SYMBOLS			
EXISTING	PROPOSED	EXISTING	PROPOSED
○	Iron pin found	— — — — —	Stockade fence
□	Monument found	— — — — —	Chain link fence
⊙	Utility pole	□	Catch basin
— — — — —	Construction Baseline	⊙	Water Shutoff
— — — — —	Edge of pavement	⊙	Sewer manhole
— — — — —	Curb	⊙	Drain manhole
— — — — —	Right of way line	⊙	Fire hydrant
— — — — —	Overhead electric	⊙	Tree
— — — — —	Water line	⊙	Guy wire
— — — — —	Storm drain	⊙	Sign
— — — — —	Underdrain	□	Building
— — — — —	Existing contour	— — — — —	Silt Fence

INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	GENERAL NOTES
3	TYPICAL SECTIONS
4-5	MISCELLANEOUS DETAILS
6	EXISTING CONDITIONS PLAN
7	INTERSECTION DEMOLITION PLAN
8	OVERALL PLAN
9	GENERAL PLAN - ROUTE 114
10	GENERAL PLAN - ROUTE 35
11	PROFILE - ROUTE 114
12	PROFILE - ROUTE 35
13	GRADING AND DRAINAGE PLAN
14	INTERSECTION LAYOUT PLAN
15	INTERSECTION GRADING PLAN
16	SIGNAL PLAN
17	SIGNAL NOTES
18	SIGNAL DETAILS
19	SIGNALING AND STRIPING PLAN



LOCATION MAP  
A PORTION OF CUMBERLAND COUNTY



**NOTE:**  
 \* MAINTENANCE OF TRAFFIC PER THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD 2009 EDITION).  
 \* CONTRACTOR SHALL USE THE B.M.P. (BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL) AS A MINIMUM STANDARD.

NOTE: THIS PLAN SET IS ISSUED FOR PERMITTING PURPOSES AND SHALL NOT BE USED FOR CONSTRUCTION.

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Drawing Name:	TITLE SHEET
Project:	Layout & Signalization of Route 114 & 35 Intersection Standish, Maine
Client:	Town of Standish 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**1**



**GENERAL NOTES**

- ALL WORK SHALL CONFORM TO CURRENT MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS AND STANDARD DETAILS, AND TOWN OF STANDISH REQUIREMENTS.
- SEE PLANS FOR INFORMATION REGARDING SURVEY AND TOPOGRAPHICAL FEATURES.
- ANY NECESSARY FINE GRADING OR RECOMPACTION OF EXISTING GRAVEL SHALL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- LOAM HAS BEEN ESTIMATED FOR 100% OF THE DISTURBED SLOPE AREA. ACTUAL PLACEMENT OF THE LOAM SHALL BE AS DIRECTED BY THE INSPECTOR.
- UNLESS OTHERWISE NOTED SEEDING METHOD NO. 1 SHALL BE UTILIZED.
- ALL MATERIAL SCHEDULES SHOWN ON THE PLANS ARE FOR GENERAL INFORMATION ONLY. THE CONTRACTOR SHALL PREPARE HIS OWN MATERIAL SCHEDULES BASED UPON HIS PLAN REVIEW. ALL SCHEDULES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS OR PERFORMING WORK.
- DO NOT EXCAVATE FOR AGGREGATE SUBBASE COURSE WHERE EXISTING MATERIAL IS SUITABLE AS DETERMINED BY THE INSPECTOR.
- IN AREAS WHERE THE INSPECTOR DIRECTS THE CONTRACTOR NOT TO EXCAVATE TO THE SUBGRADE LINE SHOWN ON THE PLANS, PAYMENT FOR REMOVING EXISTING PAVEMENT, BRICK, GRUBBING, SHAPING, DITCHING, AND COMPACTING THE EXISTING SUBBASE AND LAYERS OF NEW SUBBASE 6 INCHES OR LESS THICK WILL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- ALL WASTE MATERIAL NOT USED ON THE PROJECT SHALL BE DISPOSED OF OFF THE PROJECT IN ACCEPTABLE WASTE AREAS REVIEWED BY THE INSPECTOR. GRADING, SEEDING AND MULCHING OF WASTE AREAS SHALL BE CONSIDERED INCIDENTAL.
- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION'S BEST MANAGEMENT PRACTICES FOR EROSION CONTROL & SEDIMENT CONTROL, FEBRUARY, 2008.
- NO SEPARATE PAYMENT FOR SUPERINTENDENT OR FOREMAN WILL BE MADE FOR THE SUPERVISION OF EQUIPMENT BEING PAID FOR UNDER THE EQUIPMENT RENTAL ITEMS.
- THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED OR DAMAGED BY CONSTRUCTION ACTIVITIES TO ORIGINAL FINISH SURFACE (ROADWAY, SIDEWALK, ETC.) UNLESS NOTED OTHERWISE ON PLANS. RESTORATION OF PAVED SURFACES, SIDEWALKS AND OTHER AREAS SHALL BE INCIDENTAL TO THE PROJECT. ALL CURB DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE REPLACED IN KIND AND SHALL CONFORM TO MAINE DOT STANDARDS. COST SHALL BE INCIDENTAL TO THE PROJECT.
- "UNDETERMINED LOCATIONS" SHALL BE DETERMINED BY THE INSPECTOR.
- BACKING UP BITUMINOUS CURB IS INCIDENTAL TO THE CURB ITEMS. IN AREAS WHERE NEW BITUMINOUS CURB IS DESIGNATED TO REPLACE EXISTING, THE REMOVAL OF THE OLD BITUMINOUS CURB SHALL BE INCIDENTAL TO THE NEW CURB.
- PAVEMENT CUT LINES SHALL BE NEAT, CLEAN AND STRAIGHT AS DIRECTED BY THE RESIDENT. PAYMENT FOR CUTTING OF EXISTING PAVEMENT SHALL BE INCIDENTAL TO 403 ITEMS.
- LOAM SHALL BE PLACED TO A NOMINAL DEPTH OF 4 INCHES UNLESS OTHERWISE NOTED OR DIRECTED.
- ALL PAVED WALKS TO BE CONSTRUCTED WITH 12" AGGREGATE SUBBASE COURSE-GRAVEL AND 2 - 11/4" LIFTS OF HOT MIX ASPHALT UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE INSPECTOR.
- ALL JOINTS BETWEEN EXISTING AND PROPOSED HOT BITUMINOUS PAVEMENT SHALL BE SAWCUT AND VERTICAL. IF ANY EXISTING PAVEMENT IS REQUIRED TO BE REMOVED, PAYMENT FOR SAWCUTTING AND REMOVAL WILL BE INCIDENTAL TO PAVING ITEMS.
- BUTT JOINTS SHALL BE USED AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT MEETS EXISTING PAVEMENT. NO FEATHERING OF PAVEMENT WILL BE PERMITTED. COST OF BUTT JOINTS SHALL BE INCIDENTAL TO PAY ITEMS IN SECTION 403 - HOT BITUMINOUS PAVEMENT AND SECTION 608 - SIDEWALKS AND DRIVEWAYS.
- PROPERTY LINE AND R.O.W. MONUMENTS SHALL NOT BE DISTURBED BY CONSTRUCTION. IF DISTURBED, THEY SHALL BE RESET TO THEIR ORIGINAL LOCATIONS AT THE CONTRACTOR'S EXPENSE, BY A MAINE PROFESSIONAL LAND SURVEYOR.
- MULCH SHALL BE APPLIED IN AREAS SEEDED.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH THE EXISTING CONDITIONS PRIOR TO BIDDING.
- CLEARING IS NOT ANTICIPATED. ANY CLEARING SHALL BE AT THE DIRECTION OF THE INSPECTOR AND SHALL REMAIN WITHIN THE ROW. CLEARING SHALL NOT BE PAID FOR SEPARATELY AND WILL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- LOCATIONS OF RIGHT-OF-WAY SHOWN ON PLANS ARE APPROXIMATE ONLY.
- THE CONTRACTOR SHALL COMPLETE THE WORK INSIDE OF THE ROW, AND WILL BE RESPONSIBLE IF THEY TRESPASS ON PRIVATE PROPERTY.
- SURVEY BENCHMARKS ARE SHOWN ON THE PLANS AT THE FOLLOWING LOCATIONS:
  - RICHVILLE ROAD (ROUTE 114 SOUTHBOUND), BONNET BOLT IN HYDRANT STA. 114+34, 23.42' RT ELEV. = 284.80
  - FORT HILL ROAD (ROUTE 114 NORTHBOUND), PK FOUND IN POLE #2 STA. 18+46, 26.76' LT ELEV. = 297.48
  - NORTHEAST ROAD (ROUTE 35 SOUTHBOUND), TOP HYDRANT STA. 105+91, 30.83' RT ELEV. = 292.10
- THE CONTRACTOR SHALL NOT PARK, IMPEDE ACCESS, OR STORE EQUIPMENT/MATERIAL ON ADJACENT TOWN OR PRIVATELY OWNED LAND WITHOUT WRITTEN CONSENT FROM THE TOWN OR LAND OWNER.
- MAINTENANCE OF TRAFFIC SHALL BE PER THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND THE MAINE DOT SPECIFICATIONS AND STANDARD PLANS.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN ALL NECESSARY BARRIERS, FENCES, LIGHTS, WARNING SIGNS AND OTHER DEVICES NECESSARY TO SAFEGUARD TRAFFIC AND THE PUBLIC DURING WORKING AND NON-WORKING HOURS FOR THE DURATION OF THE PROJECT. SITE SHALL BE LEFT WITH APPROPRIATE SAFETY MEASURES IN PLACE DURING NON-WORKING HOURS. NO TRENCH SHALL BE LEFT OPEN DURING NON-WORKING HOURS. SITE SAFETY IS THE RESPONSIBILITY OF THE CONTRACTOR, DURING BOTH WORKING AND NON-WORKING HOURS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PRESERVATION OF ALL TREES AND SHRUBS ON THE PROJECT WHICH ARE NOT TO BE REMOVED.
- NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT PRIOR APPROVAL OF THE INSPECTOR.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING ALL EXISTING MAILBOXES TO ENSURE THAT THE MAIL WILL BE DELIVERED.
- GRINDING OF EXISTING PAVEMENT IN AREAS OF SAWCUT SHALL BE INCIDENTAL TO ITEM 203.20 - COMMON EXCAVATION AND WILL NOT HAVE A SEPARATE PAYMENT ITEM.
- PROTECT EXISTING FEATURES/STRUCTURES NOT CALLED OUT FOR REPLACEMENT/ALTERATION.
- THE TOWN OF STANDISH SHALL HAVE THE RIGHT AND AUTHORITY TO DETERMINE THE ACCEPTABILITY OF WORK AND MATERIALS IN PROGRESS OR COMPLETED. THE TOWN OF STANDISH SHALL HAVE THE RIGHT TO REJECT ANY WORK OR MATERIALS WHICH DO NOT CONFORM, IN ITS SOLE OPINION, TO THE PLANS OR SPECIFICATIONS.
- ALL SIGNING, SIGNAL AND STRIPING MATERIALS AND PLACEMENT SHALL CONFORM TO THE MAINE DOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND STANDARD DETAILS AND WITH THE FEDERAL HIGHWAY ADMINISTRATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY TRENCH PAVEMENT THAT HAS EXPERIENCED EXCESSIVE SETTLEMENT, CRACKING, OR OPENING OF JOINTS. REPAIRS MAY INCLUDE OVERLAY, REMOVAL OF UNACCEPTABLE MATERIALS, COMPLETE REPLACEMENT, JOINT SEALING, OR RECONSTRUCTING PAVEMENT JOINTS AS REQUIRED. THIS WORK MAY BE NECESSARY AFTER THE FINAL ACCEPTANCE OF WORK OR PRIOR TO THE ONE YEAR GUARANTEE. THIS WORK SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL ANTICIPATE THAT GROUNDWATER WILL BE ENCOUNTERED DURING CONSTRUCTION AND SHALL INCLUDE SUFFICIENT COSTS WITHIN THEIR BID TO PROVIDE DEWATERING AS NECESSARY. NO SEPARATE PAYMENT SHALL BE MADE TO THE CONTRACTOR FOR DEWATERING.

- EXISTING FACILITIES/STRUCTURES (I.E. TREES, POLES, LIGHT POLES) SHALL BE REMOVED AS SPECIFIED ON THE PLANS AND PROTECTED DURING CONSTRUCTION. CITY RETAINS THE RIGHT TO KEEP ANY AND ALL REMOVED FACILITIES/STRUCTURES. CONTRACTOR SHALL DISPOSE OF UNWANTED/UNUSED FACILITIES/STRUCTURES OFF SITE IN CONFORMANCE WITH APPLICABLE FEDERAL, STATE, AND LOCATION REGULATIONS.
- COMPACTION REQUIREMENTS:
 

LOCATION	MINIMUM COMPACTION*
BELOW PAVED AREAS	95%
BELOW SEEDED AREAS	90%

\*ALL PERCENTAGES OF COMPACTION SHALL BE OF MAXIMUM DRY DENSITY AT THE OPTIMUM MOISTURE CONTENT AS DETERMINED AND CONTROLLED IN ACCORDANCE WITH ASTM D-1557.
- ALL EXISTING CURB AND COBBLESTONES NOT REUSED AS PART OF THIS PROJECT SHALL BE DELIVERED TO A TOWN DESIGNATED LOCATION, AT NO ADDITIONAL COST TO THE TOWN.

**UTILITY NOTES**

- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT NO UTILITIES (UNDERGROUND OR OVERHEAD) ARE SHOWN ON THESE PLANS.
- ANY ADDITIONAL UTILITY WORK NOT SPECIFIED ON THE PLANS SHALL BE COMPLETED BY THE RESPECTIVE UTILITY COMPANY.
- CONTRACTOR SHALL COORDINATE ANY DISRUPTION OF PRIVATE UTILITY SERVICES WITH LAND OWNER AT LEAST 2 DAYS (48 HOURS) PRIOR TO SCHEDULED DISRUPTION.
- TEST PITS, IF REQUIRED, SHALL BE COMPLETED AT LEAST TWO WEEKS PRIOR TO THE START OF CONSTRUCTION OR ORDERING OF MATERIALS. THE CONTRACTOR SHALL PROMPTLY PROVIDE TEST PIT INFORMATION TO THE RESIDENT FOR REVIEW, AND SHALL NOTIFY THE RESIDENT OF ANY POTENTIAL UTILITY CROSSING CONFLICTS.
- THE CONTRACTOR SHALL CALL THE APPROPRIATE UTILITY COMPANY AND DIG SAFE (888-344-7233) AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. THE FOLLOWING UTILITY CONTACTS (SHOWN TO THE RIGHT) MAY HAVE FACILITIES WITHIN THE PROJECT LIMIT.
- THE CONTRACTOR SHALL CONDUCT A UTILITY COORDINATION MEETING A MINIMUM OF TWO WEEKS PRIOR TO STARTING ON-SITE UNLESS OTHERWISE APPROVED BY THE TOWN.
- WATER AND SEWER SERVICE SHALL BE MAINTAINED AT ALL TIMES. COORDINATE WITH PORTLAND WATER DISTRICT AND THE TOWN OF STANDISH FOR LOCATION OF WATER AND SEWER SERVICES.
- ELECTRICAL AND TELECOMMUNICATIONS SERVICE PROVIDED VIA OVERHEAD LINES, UNLESS OTHERWISE SHOWN. CONTRACTOR SHALL USE CAUTION WHEN WORKING NEAR EXISTING OVERHEAD LINES.
- CLEAN AND/OR FLUSH SEDIMENT AND DEBRIS FROM ALL MANHOLES, CATCH BASINS AND ASSOCIATED PIPING AFTER THE WORK HAS BEEN COMPLETED. ACCUMULATED SEDIMENT SHALL BE REMOVED AND PROPERLY DISPOSED OF.
- STATIONING, PIPE LENGTHS, PIPE SLOPES AND PIPE INVERT CALCULATIONS ARE MEASURED ALONG THE PIPE CENTERLINE TO THE INSIDE WALL OF MANHOLE AND CATCH BASIN STRUCTURES.
- CATCH BASIN OFFSETS AND RIM ELEVATIONS ARE MEASURED TO THE CENTER OF THE GRATE.
- PROPOSED CATCH BASIN AND DRAIN MANHOLE (DMH) SYMBOLS REPRESENT NEW STRUCTURES. REFER TO DETAILS AND SPECIFICATIONS FOR INFORMATION ON FRAMES AND GRATES, COVERS, SHAPE, STYLE AND DIMENSIONS. CATCH BASINS SHALL BE SET IN ACCORDANCE WITH CITY DETAILS AND SPECIFICATIONS.
- ON ALL "ALTER", "MODIFY" AND "REMOVE" STRUCTURES, THE CONTRACTOR SHALL REMOVE ABANDONED PIPES OR PLUG AND FILL WITH FLOWABLE FILL.
- THE COST OF REMOVING EXISTING STRUCTURES AND/OR PIPES SHALL BE INCIDENTAL TO THE COST OF THE REPLACEMENT STRUCTURE AND/OR PIPE WHEN REPLACED IN THE SAME VICINITY, REFER TO SPECIFICATION SECTION 202.08.
- THE FOLLOWING SHALL BE INCIDENTAL TO THE 603 & 604 PAY ITEMS:
  - CUTTING OF PIPE AND/OR CONNECTIONS NECESSARY TO CONSTRUCT NEW STORM DRAIN & SEWER PIPE AND APPURTENANCES
  - WORK & MATERIALS NECESSARY TO CONNECT NEW OR EXISTING PIPES TO CATCH BASINS OR MANHOLES
  - CHANGES TO FLOW LINES/PROFILE GRADES/PIPE INVERTS OF ONE(1) FOOT OR LESS
- AT LOCATIONS WHERE EXISTING CATCH BASINS ARE BEING REMOVED AND NEW CATCH BASINS ADDED IN A DIFFERENT LOCATION, CONTRACTOR SHALL REGRADE THE AREA WHERE THE OLD BASIN IS BEING REMOVED TO BLEND IN SMOOTHLY WITH ADJACENT STREET GRADES. AT THE LOCATION OF THE NEW CATCH BASIN, CONTRACTOR SHALL CONSTRUCT APPROPRIATE DEPRESSION PER THE TOWN OF STANDISH DETAILS AND STANDARDS.

**EROSION CONTROL NOTES**

- LAND DISTURBING ACTIVITIES SHALL BE ACCOMPLISHED IN A MANNER AND SEQUENCE THAT CAUSES THE LEAST PRACTICAL DISTURBANCE OF THE SITE.
- PRIOR TO BEGINNING ANY CLEARING/LAND DISTURBING ACTIVITIES, THE CONTRACTOR SHALL INSTALL THE PERIMETER SILT FENCES.
- SILTATION FENCE SHALL BE INSTALLED DOWNGRADIENT OF ANY DISTURBED AREAS TO TRAP RUNOFF- BORNE SEDIMENTS UNTIL GRASS AREAS ARE REVEGETATED. THE SILT FENCE SHALL BE INSTALLED PER THE DETAILS PROVIDED ON THIS PLAN AND INSPECTED BEFORE AND IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. REPAIRS SHALL BE MADE IF THERE ARE ANY SIGNS OF EROSION OR SEDIMENTATION BELOW THE FENCE LINE. IF THERE ARE SIGNS OF UNDERCUTTING AT THE CENTER OR THE EDGES, OR IMPOUNDING OF LARGE VOLUMES OF WATER BEHIND THE FENCE, THE BARRIER SHALL BE REPLACED WITH A STONE CHECK DAM.
- SILT FENCING WITH A MINIMUM STAKE SPACING OF 6 FEET SHALL BE USED, UNLESS THE FENCE IS SUPPORTED BY WIRE FENCE REINFORCEMENT OF MINIMUM 14 GAUGE AND WITH A MAXIMUM MESH SPACING OF 6 INCHES, IN WHICH CASE STAKES MAY BE SPACED A MAXIMUM OF 10 FEET APART. THE BOTTOM OF THE FENCE SHALL BE ANCHORED.
- SILT REMOVED FROM AROUND INLETS AND BEHIND THE SILT FENCES SHALL BE PLACED ON A TOPSOIL STOCKPILE AND MIXED INTO IT FOR LATER USE IN LANDSCAPING OPERATIONS.
- ALL GROUND AREAS GRADED FOR CONSTRUCTION WILL BE GRADED, LOAMED AND SEEDED AS SOON AS POSSIBLE. PERMANENT SEED MIXTURE SHALL CONFORM TO THE SEEDING PLAN CONTAINED IN THE EROSION CONTROL PLANS AND NOTES.
- ALL NON-PAVED AREAS DISTURBED DURING CONSTRUCTION SHALL BE LOAMED AND SEEDED, UNLESS OTHERWISE SHOWN ON PLANS OR DIRECTED BY THE TOWN.
- LOAM AND SEED IS INTENDED TO SERVE AS THE PRIMARY PERMANENT REVEGETATIVE MEASURE FOR ALL DENUDED AREAS NOT PROVIDED WITH OTHER EROSION CONTROL MEASURES, SUCH AS RIPRAP.
- ROUTE 35, ROUTE 114, AND ANY SIDE STREETS AFFECTED BY THIS PROJECT SHALL BE SWEEPED TO CONTROL MUD, DUST, AND THE TRACKING OF MATERIAL OFF THE SITE AND ONTO THE SURROUNDING ROADWAYS, AS NECESSARY.
- WATER AND/OR CALCIUM CHLORIDE SHALL BE FURNISHED AND APPLIED IN ACCORDANCE WITH MDOT SPECIFICATIONS - SECTION 637 - DUST CONTROL.
- THE CONTRACTOR IS CAUTIONED THAT FAILURE TO COMPLY WITH THE SEQUENCE OF CONSTRUCTION, EROSION/SEDIMENT CONTROL PLAN, AND OTHER PERMIT REQUIREMENTS MAY RESULT IN MONETARY PENALTIES. THE CONTRACTOR SHALL BE ASSESSED ALL SUCH PENALTIES AT NO COST TO THE TOWN OR PERMITTEE.

**STANDARDS FOR TIMELY STABILIZATION OF CONSTRUCTION SITES**

- THE CONTRACTOR SHALL STABILIZE THE DISTURBED SLOPES WITH LOAM AND SEED AND EROSION CONTROL BLANKET (NORTH AMERICAN GREEN SC150 OR APPROVED EQUIVALENT.) INSTALLED AND ANCHORED IN ACCORDANCE WITH MANUFACTURER'S REQUIREMENTS.
- DITCH FLOW LINE SHALL BE STABILIZED WITH NORTH AMERICAN GREEN SC150 OR EQUIVALENT INSTALLED AND POSITIONED IN ACCORDANCE WITH MANUFACTURER'S REQUIREMENTS.

**UTILITY CONTACTS**

- BROOKFIELD RENEWABLE ENERGY PARTNERS, L.P.  
ATTN: ERNEST DELUCA  
150 MAIN STREET  
LEWISTON, ME 04240  
(207)755-5619  
m1191h@att.com
- CENTRAL MAINE POWER COMPANY  
ATTN: TIMOTHY LANEY  
57 OLD WINTHROP ROAD  
AUGUSTA, ME 04330  
(207)629-9555  
timothy.laney@cmpco.com
- TOWN OF STANDISH  
ATTN: GORDON BILLINGTON  
175 NORTHEAST ROAD  
STANDISH, ME 04084  
(207)642-3461  
standishtownmanager@roadrunner.com
- CHARTER COMMUNICATIONS  
ATTN: CHARTER LETTER 1  
118 JOHNSON ROAD  
PORTLAND, ME 04102  
(207)620-3410  
dipormenconstleadership@charter.com
- CONSOLIDATED COMMUNICATIONS OF MAINE COMPANY  
ATTN: CONSOLIDATED LETTER 1  
5 DAVIS FARM ROAD  
PORTLAND, ME 04103  
(207)878-0854  
mdot\_request@fairpoint.com
- GW/BIDDEFORD INTERNET CORP.  
ATTN: KIEITH, ELLIS  
43 LANDRY STREET  
BIDDEFORD, ME 04005  
(877)214-2009  
kiethellis@staffgwi.net
- MAINE DOT RAILROAD  
ATTN: GREG GAY  
18 STATE HOUSE STATION  
AUGUSTA, ME 04333-0016  
(207)592-1766  
greg.gay@maine.gov
- MAINE FIBER COMPANY  
ATTN: MICHAEL ELLINGWOOD  
491 LISBON STREET  
LEWISTON, ME 04240  
(207)333-3471  
mellingwood@firstlight.net
- OTT COMMUNICATIONS (SOUTH)  
ATTN: JIM KNIGHT  
56 CAMPUS DRIVE  
NEW GLOUCESTER, ME 04240  
(207)688-8284  
jim.knight@otelco.com

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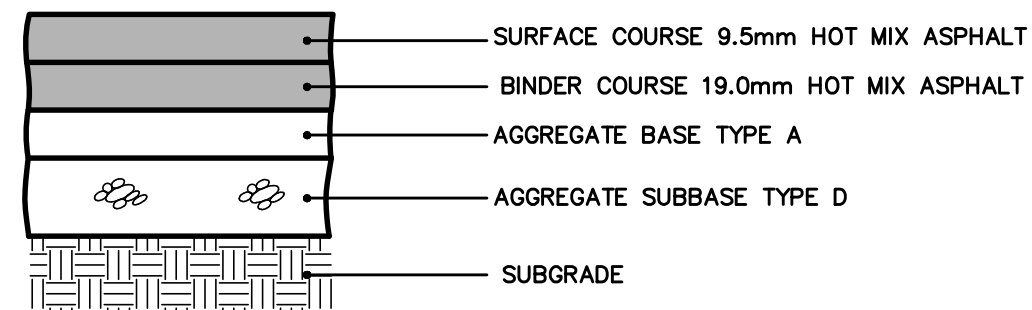


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Drawing Name:	<b>GENERAL NOTES</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

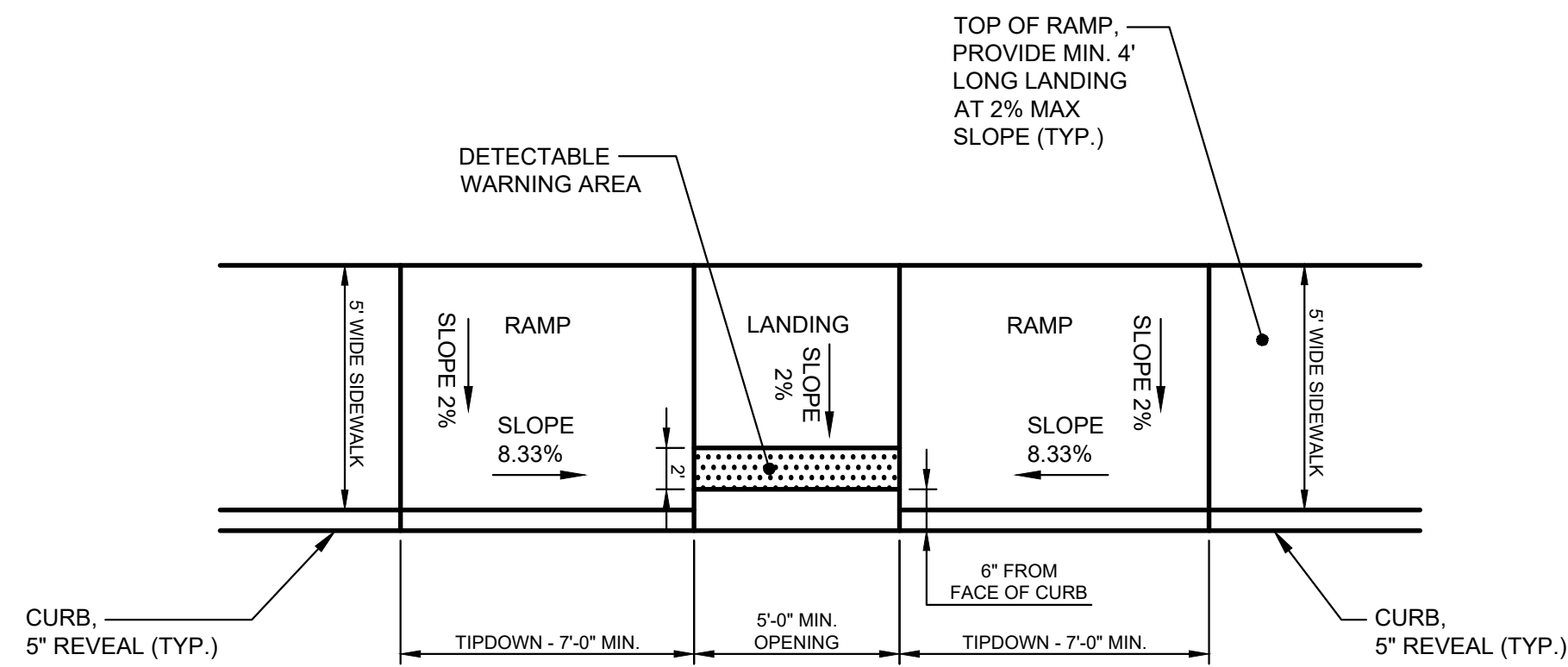
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NOTE:  
COMPACT SUBGRADE TO 95% MAXIMUM DRY DENSITY IN ACCORDANCE WITH ASTM D-1557

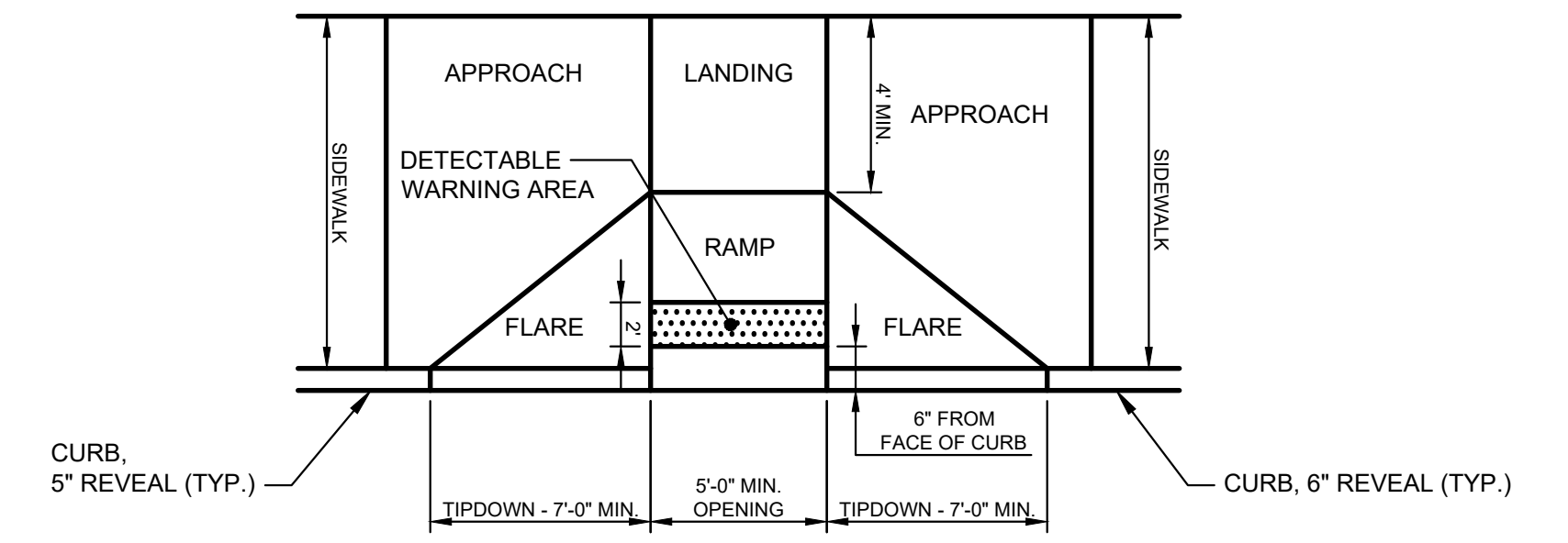
LAYERS	HEAVY DUTY PAVEMENT
SURFACE COURSE 9.5mm HMA	1.25"
BINDER COURSE 19.0mm HMA	2.75"
AGGREGATE BASE TYPE A	4"
AGGREGATE SUBBASE TYPE D	18"

**BITUMINOUS PAVEMENT SECTION**  
NOT TO SCALE



NOTES:  
1. ALL RAMPS SHALL COMPLY WITH ADA STANDARDS.  
2. CURB ADJACENT TO ADA RAMP OPENING SHALL BE FLUSH WITH STREET.

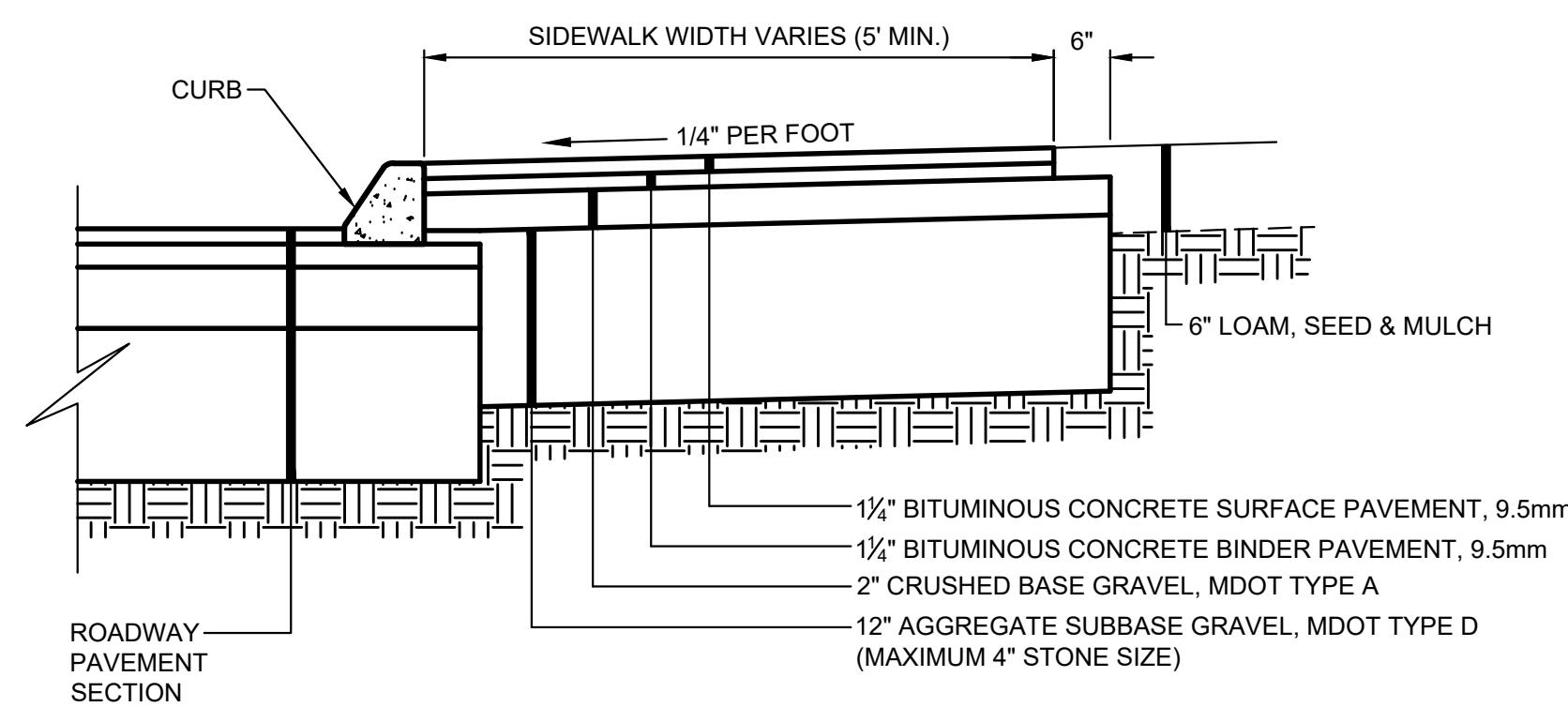
**PARALLEL ADA RAMP LAYOUT FOR NARROW SIDEWALK WITHOUT ESPLANADE**  
NOT TO SCALE



NOTES:  
1. ALL RAMPS SHALL COMPLY WITH ADA STANDARDS.  
2. CURB ADJACENT TO ADA RAMP OPENING SHALL BE FLUSH WITH STREET.

DESIGN ELEMENT	SLOPE IN DIRECTION OF TRAVEL	CROSS SLOPE
APPROACH	8.33% MAXIMUM	2%
LANDING	2%	2%
RAMP	8.33% MAXIMUM	MATCH STREET GRADE
FLARE	10% MAX. AT CURB FACE	-
SIDEWALK	MATCH STREET GRADE	2%

**PERPENDICULAR ADA RAMP LAYOUT FOR WIDE SIDEWALK WITHOUT ESPLANADE**  
NOT TO SCALE



**TYPICAL BITUMINOUS SIDEWALK**  
NOT TO SCALE

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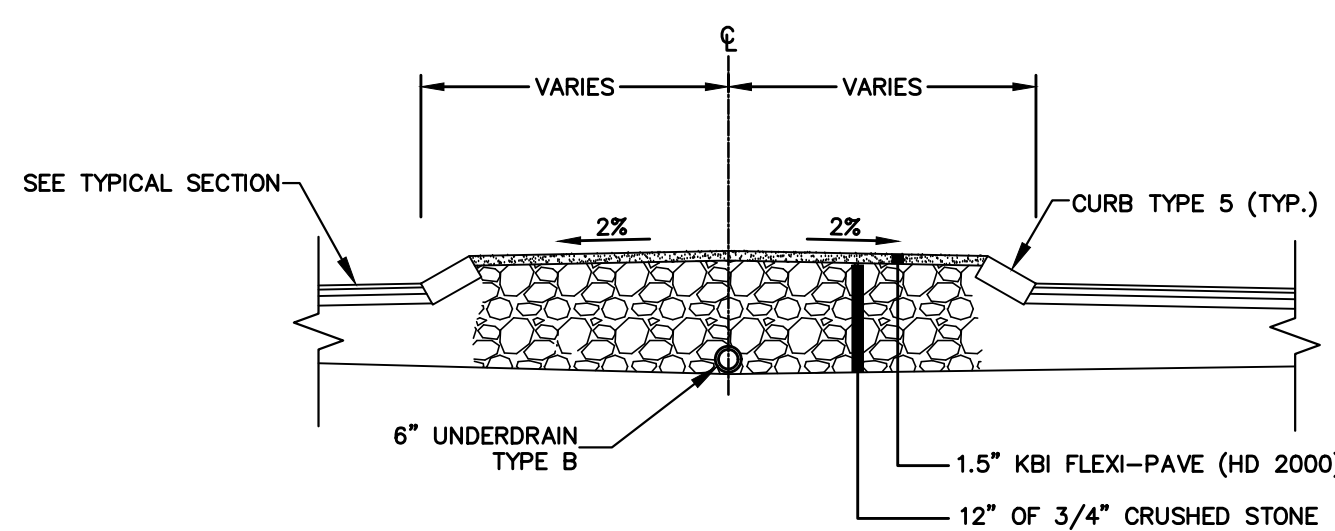
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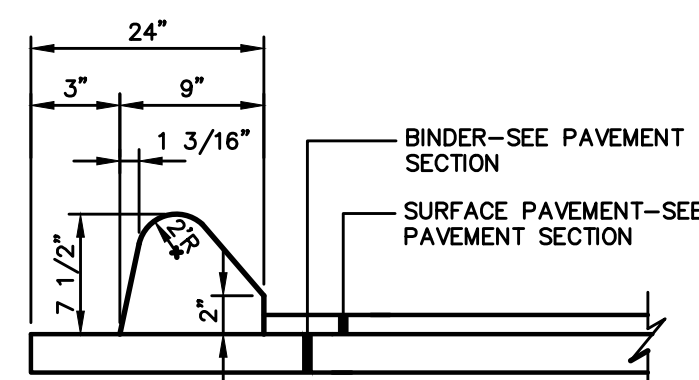
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Drawing Name:	<b>TYPICAL SECTIONS</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

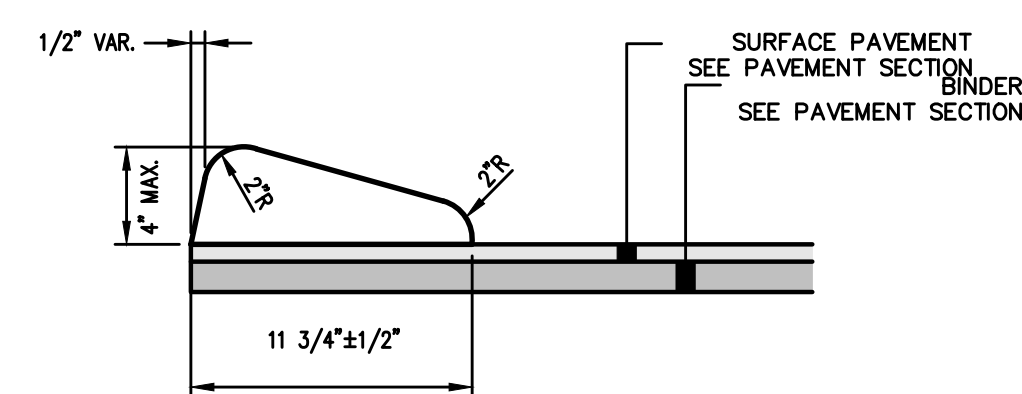
Drawing No.	<b>3</b>
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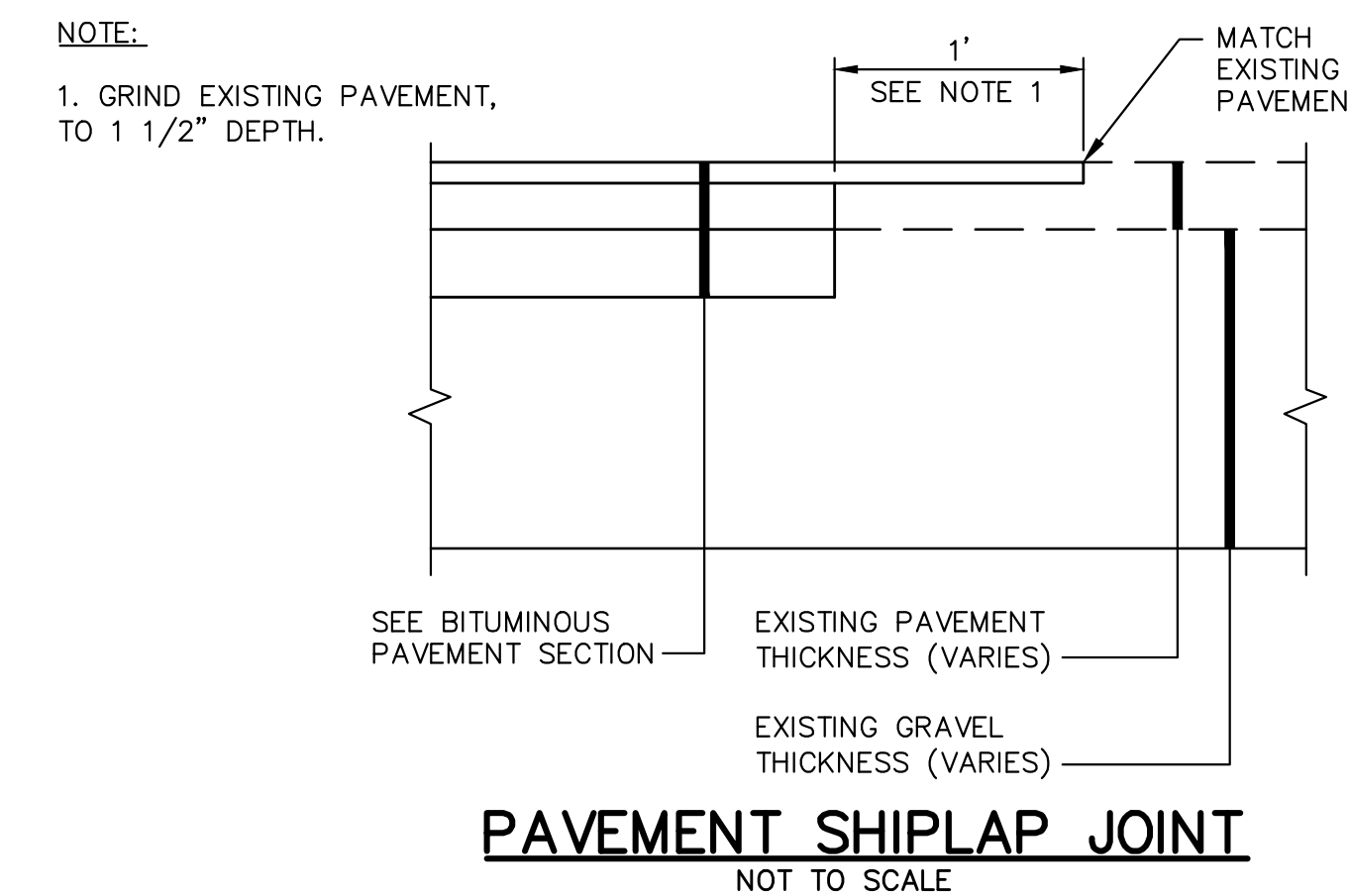
**MEDIAN ISLAND DETAIL**  
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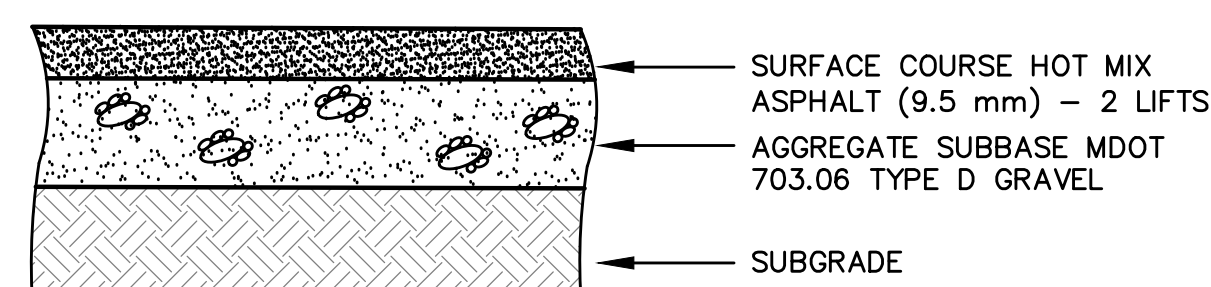
**TYPE 3, MOLD 2 CURB DETAIL**  
NOT TO SCALE



**TYPE 3, MOLD 3 CURB DETAIL**  
NOT TO SCALE



**PAVEMENT SHIPLAP JOINT**  
NOT TO SCALE

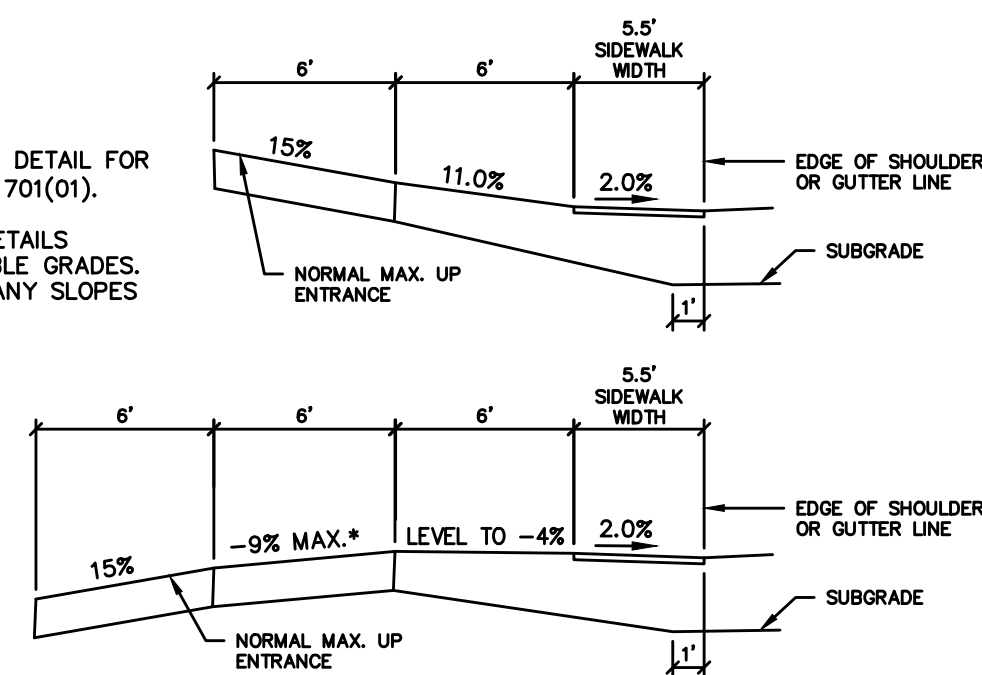


NOTE: COMPACT SUBGRADE TO 95% MAXIMUM DRY DENSITY IN ACCORDANCE WITH ASTM D-1557

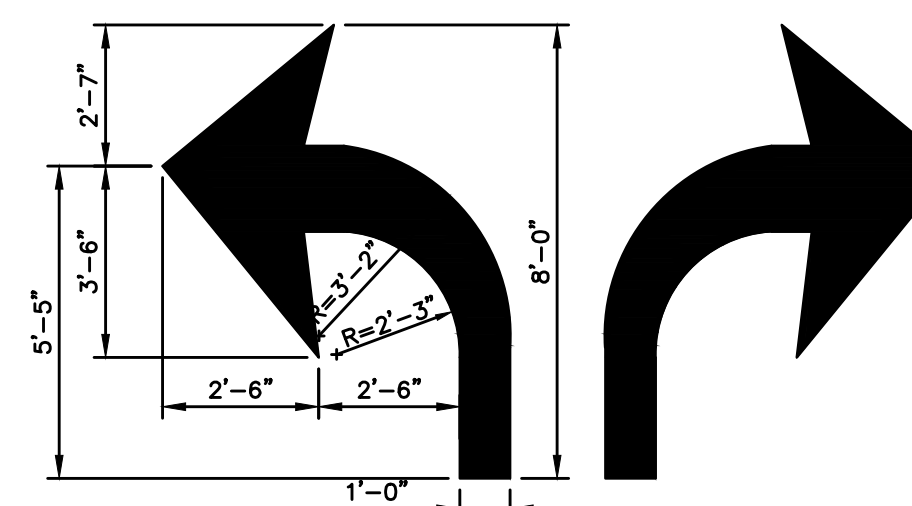
LAYERS	RESIDENTIAL DRIVE	COMMERCIAL DRIVE
HOT MIX ASPHALT (9.5mm)	2.5"	3"
AGGREGATE SUBBASE MDOT 703.06 TYPE D GRAVEL	12"	11"

**PAVED DRIVEWAY SECTION**  
NOT TO SCALE

NOTE:  
1. REFER TO MAINE DOT STANDARD DETAIL FOR ENTRANCES ON SIDEWALK SECTION 701(01).  
2. THE PERCENTAGES ON THESE DETAILS REPRESENT THE MAXIMUM ALLOWABLE GRADES. THE GRADE DIFFERENCE BETWEEN ANY SLOPES SHALL NEVER EXCEED 9%.

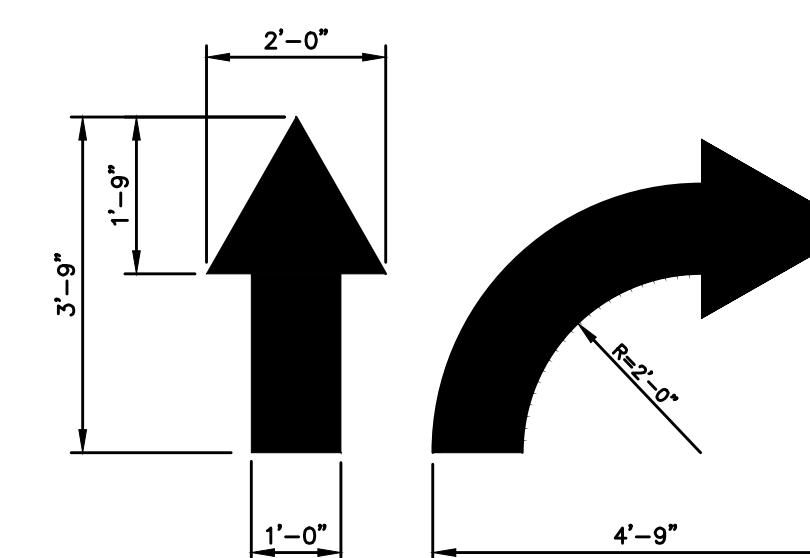


**TYPICAL DRIVEWAY SLOPES DETAIL**  
NOT TO SCALE



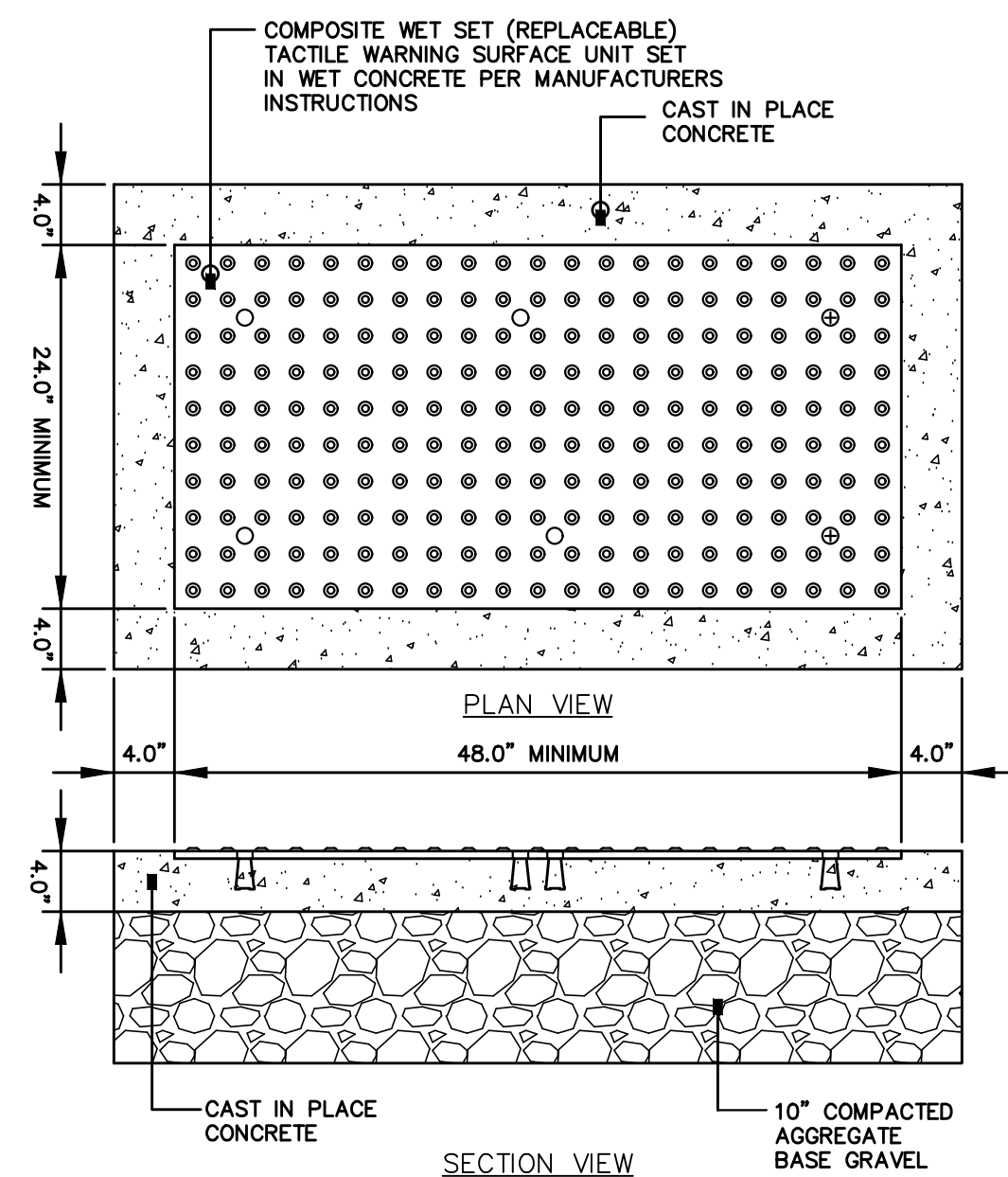
NOTE:  
ALL TRAFFIC FLOW ARROWS TO BE SOLID WHITE REFLECTIVE TRAFFIC PAINT AS PER DIMENSIONS.

**TRAFFIC FLOW ARROWS**  
NOT TO SCALE

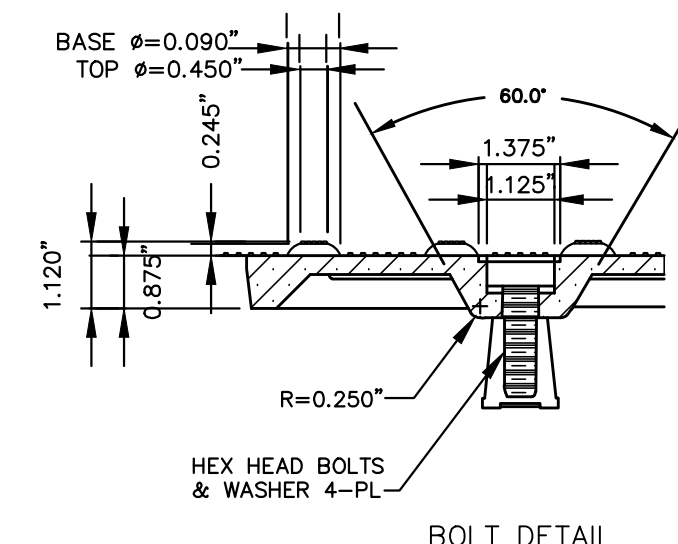


NOTE:  
ALL TRAFFIC FLOW ARROWS TO BE SOLID WHITE REFLECTIVE TRAFFIC PAINT AS PER DIMENSIONS.

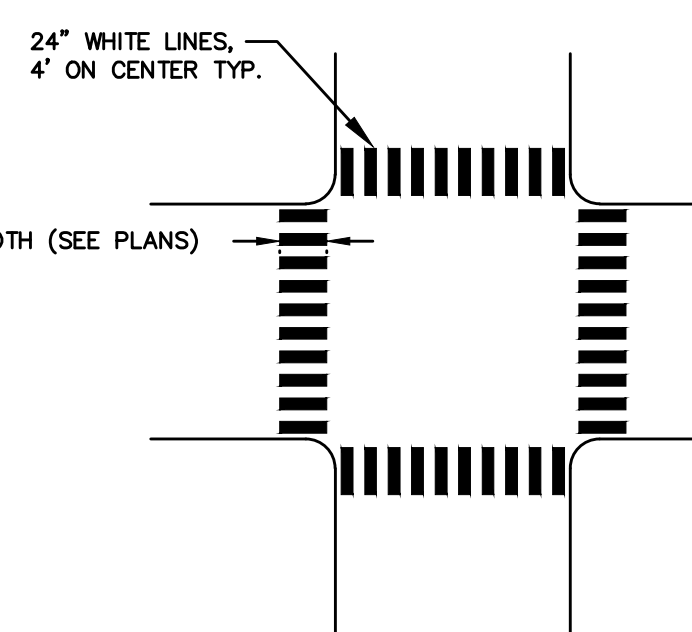
**TRAFFIC FLOW ARROWS**  
NOT TO SCALE



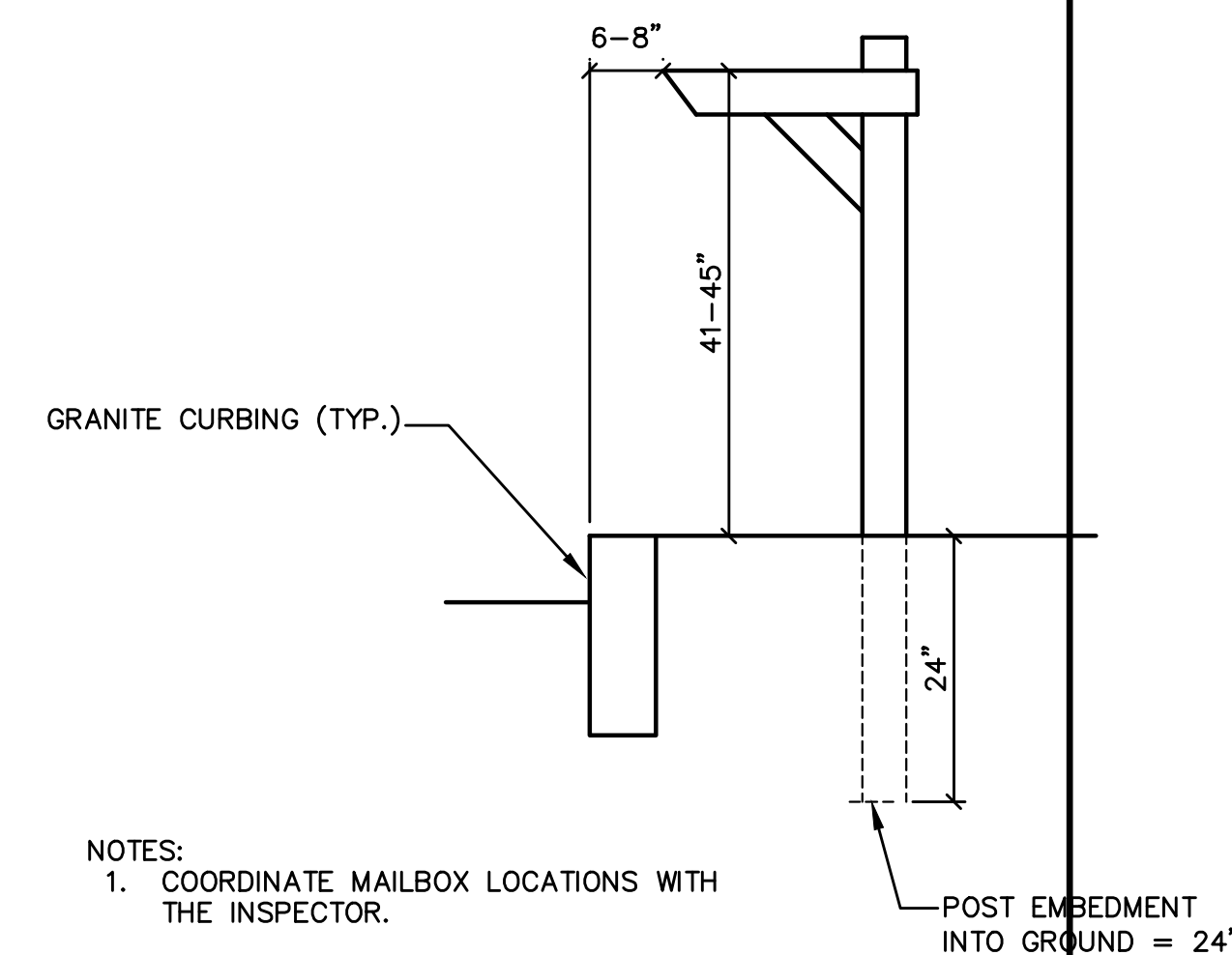
- NOTES:
- ALL DETECTABLE WARNING PLATES SHALL BE UNCOATED CAST IRON. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
  - CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR MDOT CLASS A STRUCTURAL CONCRETE, MINIMUM COMPRESSIVE STRENGTH 4,000 PSI. THE CONCRETE SHALL BE SEALED PRIOR TO SETTING PANELS. THE EXPOSED CONCRETE BORDER SHALL RECEIVE A GROOVED EDGE BETWEEN THE TILE AND CONCRETE, ALONG WITH A UNIFORM BROOM FINISH PERPENDICULAR TO THE FLOW OF PEDESTRIAN TRAFFIC.
  - TRUNCATED DOMES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. TRUNCATED DOME BRICKS AND GRANITE PAVERS ARE NOT ALLOWED.
  - SIZE: THE DETECTABLE WARNING PLATES SHALL EXTEND 24 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE PLATE WIDTH PLUS CONCRETE SHALL EXTEND THE WIDTH OF THE CURB RAMP, LANDING OR BLENDED TRANSITION TO THE STREET.
  - ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.



**SIDEWALK RAMP DETECTABLE WARNING TILE**  
NOT TO SCALE

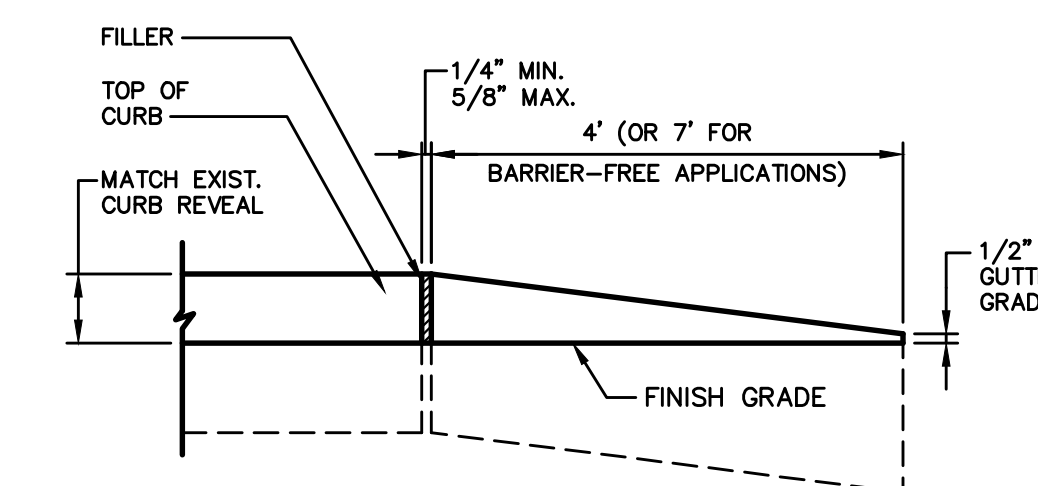


**CROSSWALK MARKINGS**  
NOT TO SCALE



NOTES:  
1. COORDINATE MAILBOX LOCATIONS WITH THE INSPECTOR.

**MAILBOX DETAIL**  
NOT TO SCALE



**TIPDOWN CURB**  
NOT TO SCALE

NOTE: THIS PLAN SET IS ISSUED FOR PERMITTING PURPOSES AND SHALL NOT BE USED FOR CONSTRUCTION.

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Rev.	Date	Revision

Issued For	Date	By

Design:	HWH	Draft:	LAN	Date:	OCT 2023
Checked:	WCH	Scale:	NTS	Job No.:	3863
File Name:	3863-DET.dwg				

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Drawing Name:	<b>MISCELLANEOUS DETAILS</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.	<b>4</b>
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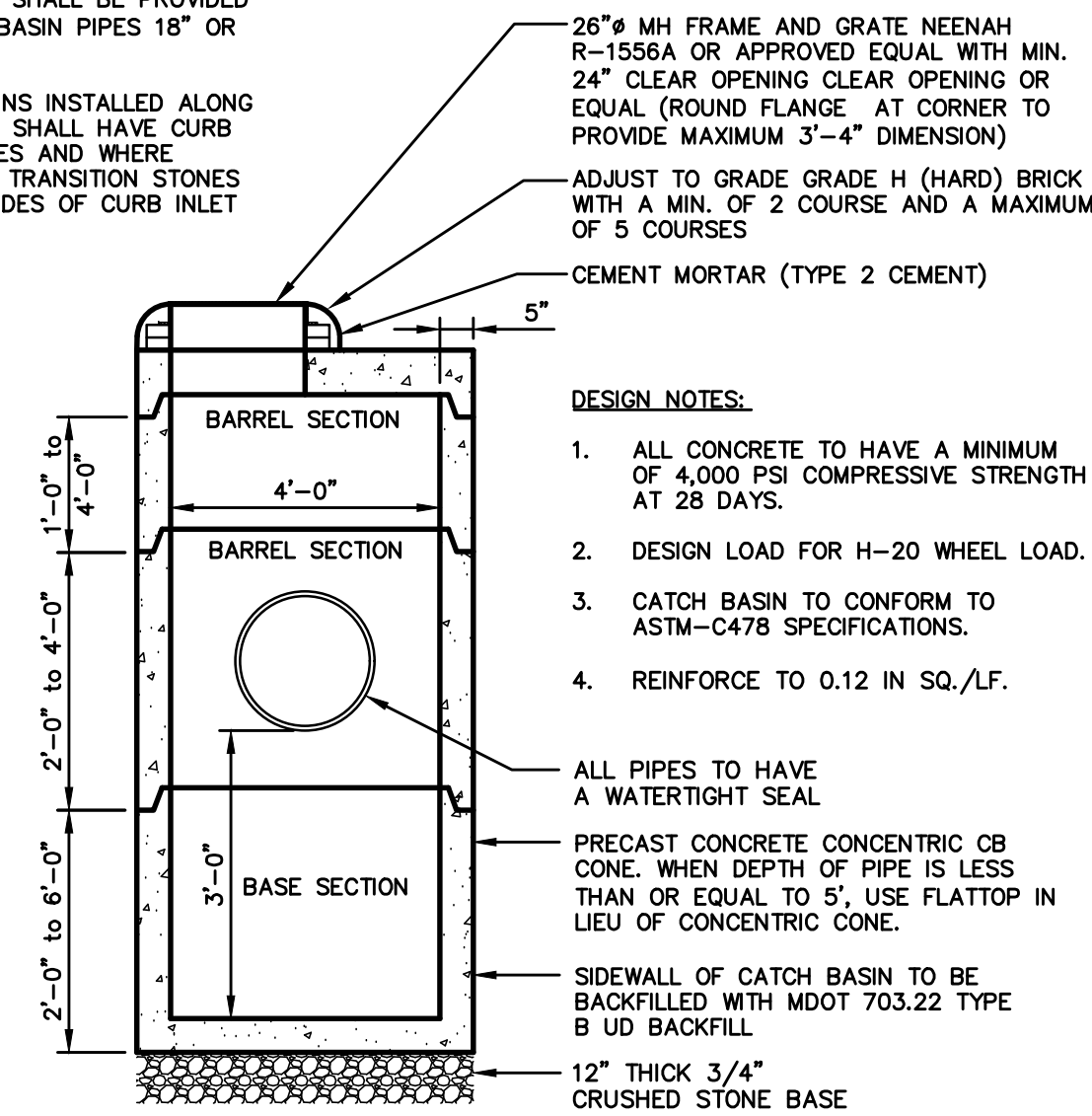


**GENERAL NOTES FOR MANHOLES & CATCH BASINS**

- ALL CONCRETE SHALL HAVE A MINIMUM ULTIMATE COMPRESSIVE STRENGTH OF 4000 LBS. PER SQ. INCH AT THE END OF 28 DAYS, UNLESS OTHERWISE NOTED.
- MANHOLES MAY BE CONSTRUCTED OF PRECAST REINFORCED CONCRETE, UNLESS OTHERWISE APPROVED.
- ALL SANITARY MANHOLE COVERS SHALL HAVE "SEWER" CAST INTO THE COVER. ALL STORMWATER/DRAIN MANHOLE COVERS SHALL HAVE "DRAIN" CAST INTO THE COVER.
- ALL MANHOLE RISERS SHALL BE 24" CLEAR OPENING OR APPROVED EQUAL.
- SEWER BRICK SHALL CONFORM TO ASTM SPEC. DESIGNATE ON C-32-63, GRADE MA AND SA.
- ALL SANITARY MANHOLES SHALL HAVE A WATERPROOFING COATING APPLIED TO THE EXTERIOR SURFACE.
- CASTINGS SHALL CONFORM TO ASTM DESIGNATION A48-CLASS 35.
- EXISTING MANHOLES, CATCH BASINS, FRAMES, AND COVERS SHALL BE SALVAGED BY THE CONTRACTOR, AND SHALL REMAIN THE PROPERTY OF THE TOWN OF STANDISH.
- ALL CATCH BASIN OUTLETS SHALL BE INSTALLED WITH A SEDIMENT HOOD.

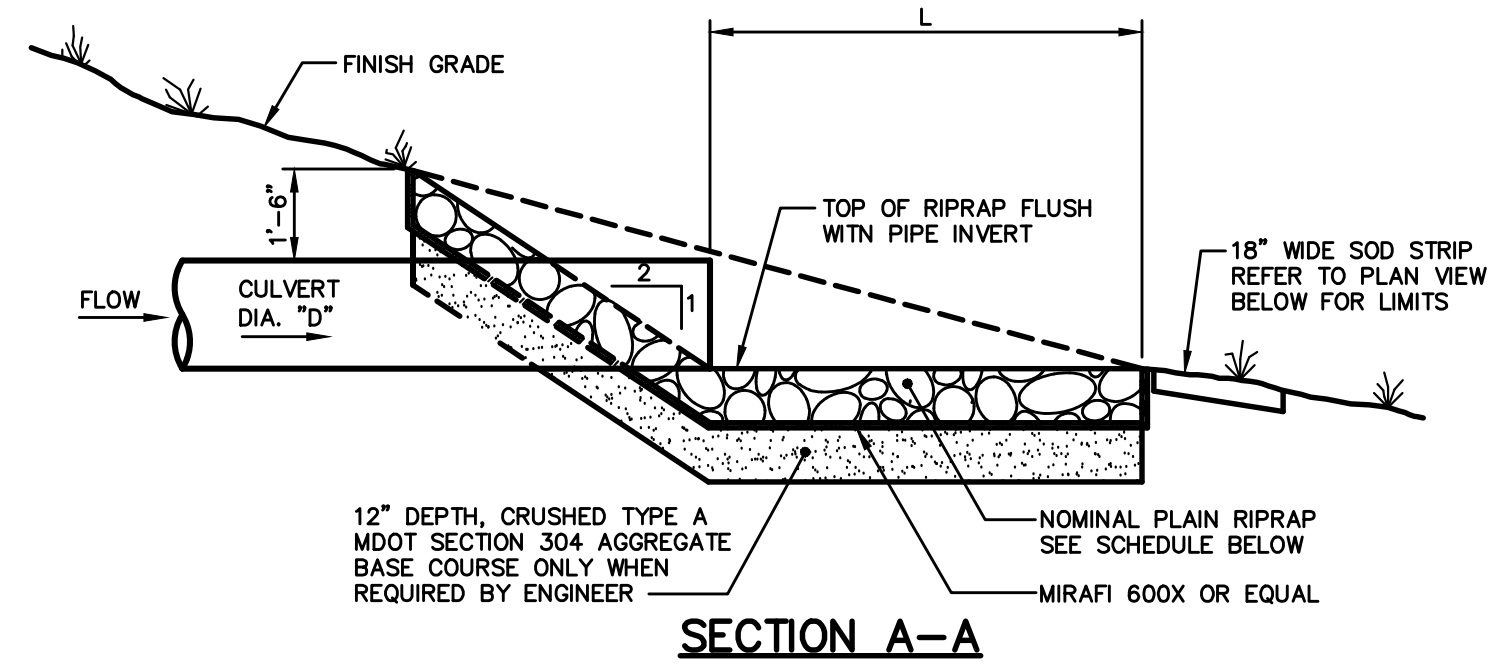
**NOTES:**

- "THE SNOT" OR APPROVED EQUIVALENT SHALL BE PROVIDED ON CATCH BASIN PIPES 18" OR LESS.
- CATCH BASINS INSTALLED ALONG CURB LINES SHALL HAVE CURB INLET STONES AND WHERE NECESSARY TRANSITION STONES ON BOTH SIDES OF CURB INLET STONE.



**4'-0" PRECAST CATCH BASIN**

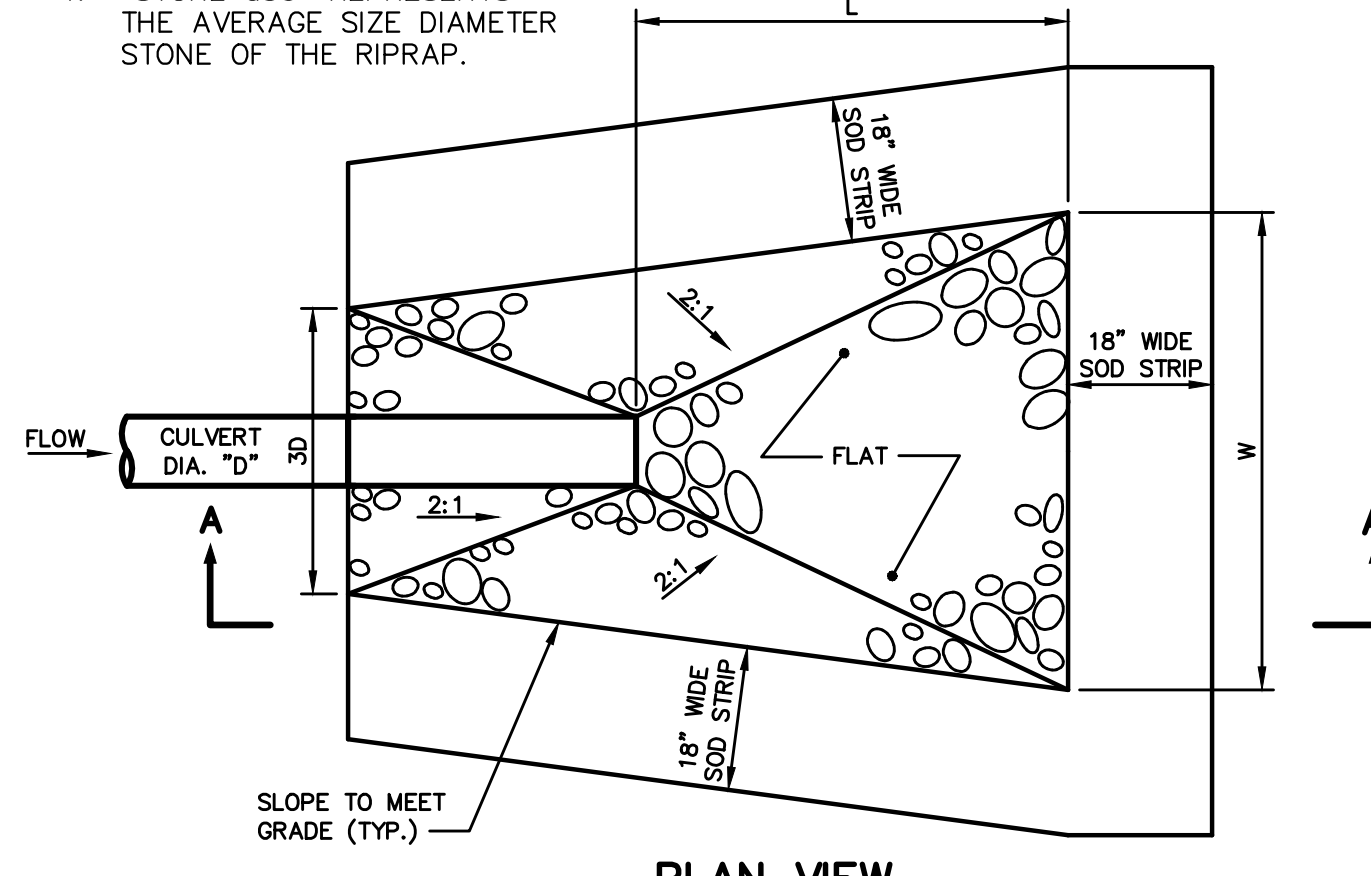
NOT TO SCALE



SCHEDULE				
CULVERT DIAMETER (D)	LENGTH (L)	WIDTH (W)	STONE d50	RIPRAP THICKNESS
12"	6'	4'	8"	18"
15"	7'	4'	8"	18"
18"	8'	4'	8"	18"
24"	12'	10'	8"	18"

**NOTES:**

- "STONE d50" REPRESENTS THE AVERAGE SIZE DIAMETER STONE OF THE RIPRAP.

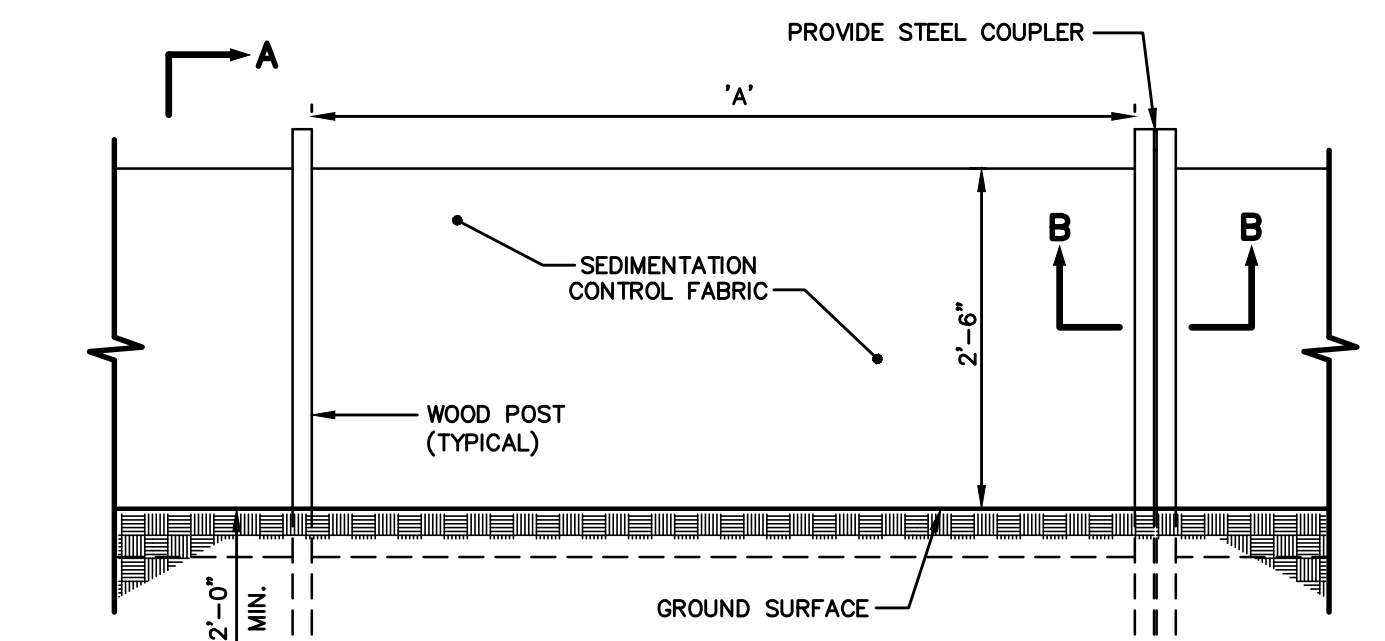
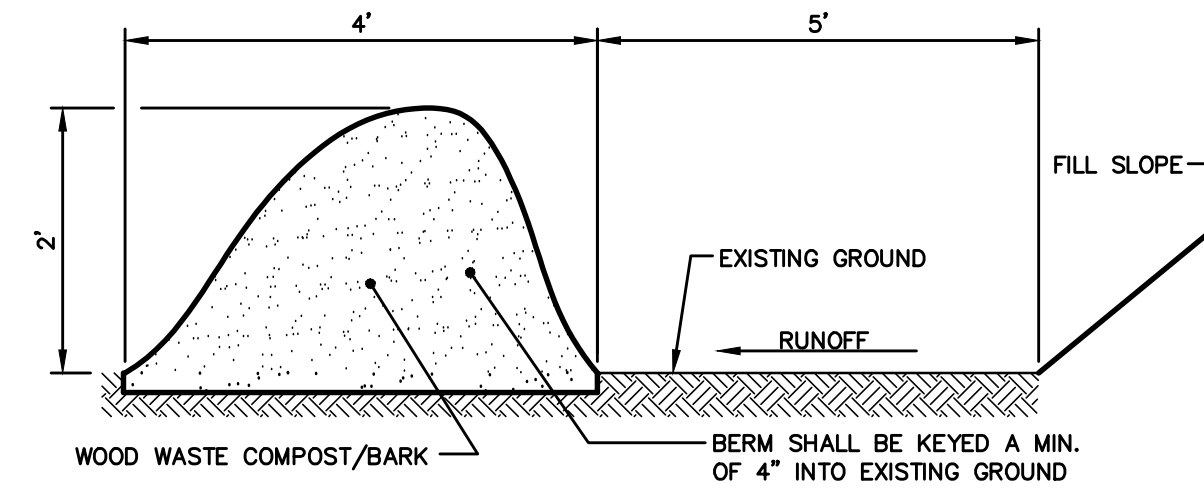


**CULVERT OUTLET APRON**

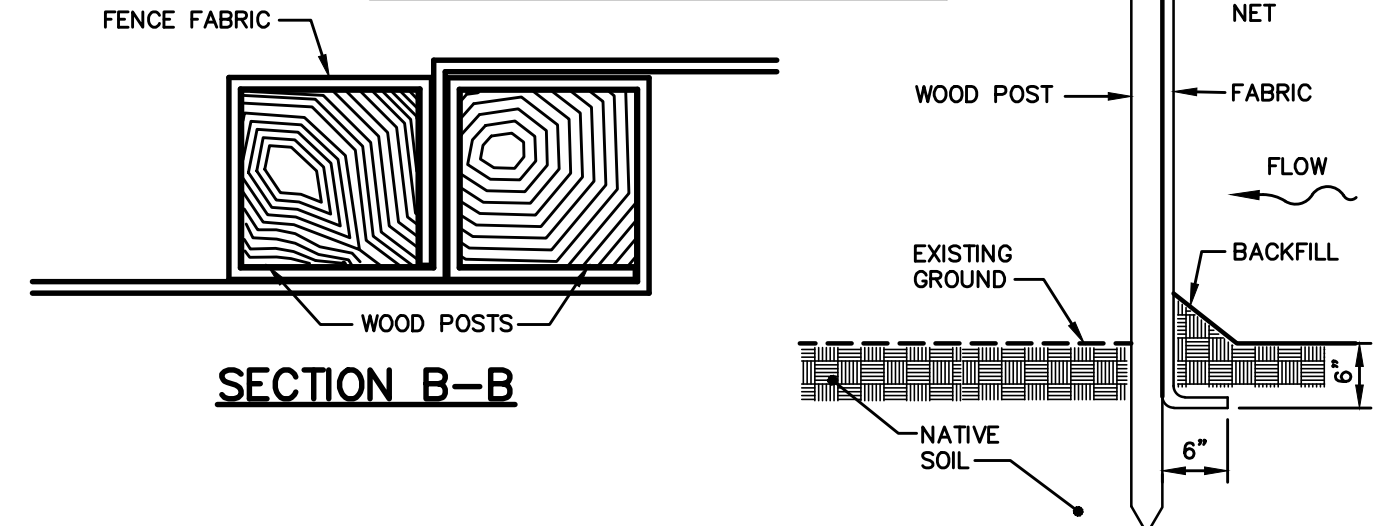
NOT TO SCALE

**NOTES:**

- THE WOOD WASTE COMPOST/BARK MIX SHALL CONFORM TO THE FOLLOWING STANDARDS:
  - A. MOISTURE CONTENT - 30-60%.
  - B. pH - 5.0 - 8.0.
  - C. SCREEN SIZE - 100% LESS THAN 3", MAX. 70% LESS THAN 1".
  - D. NO LESS THAN 40% ORGANIC MATERIAL (DRY WEIGHT) BY LOSS OF IGNITION.
  - E. NO STONES LARGER THAN 2" IN DIAMETER.
  - F. SILTS, CLAYS OR SUGAR SANDS ARE NOT ACCEPTABLE IN THE MIX.
- THE COMPOST BERM SHALL BE PLACED, UNCOMPACTED, ALONG A RELATIVELY LEVEL CONTOUR.
- THE WOOD WASTE COMPOST/BARK FILTER BERM MAY BE USED IN LIEU OF SILTATION FENCE, AT THE TOE OF SHALLOW SLOPES, ON FROZEN GROUND, LEDGE OUT CROPS, VERY ROOTED FORESTED AREA OR AT THE EDGE OF GRAVEL PARKING AREAS.
- BERMS SHALL REMAIN IN PLACE UNTIL UPSTREAM AREA IS COMPLETED OR 70% CATCH OF VEGETATION IS ATTAINED. BERMS SHALL BE REMOVED BY SPREADING SUCH THAT NATIVE EARTH CAN BE SEEN BELOW.
- WOODWASTE COMPOST BARK FILTER SHALL NOT BE USED IN WETLAND AREAS.



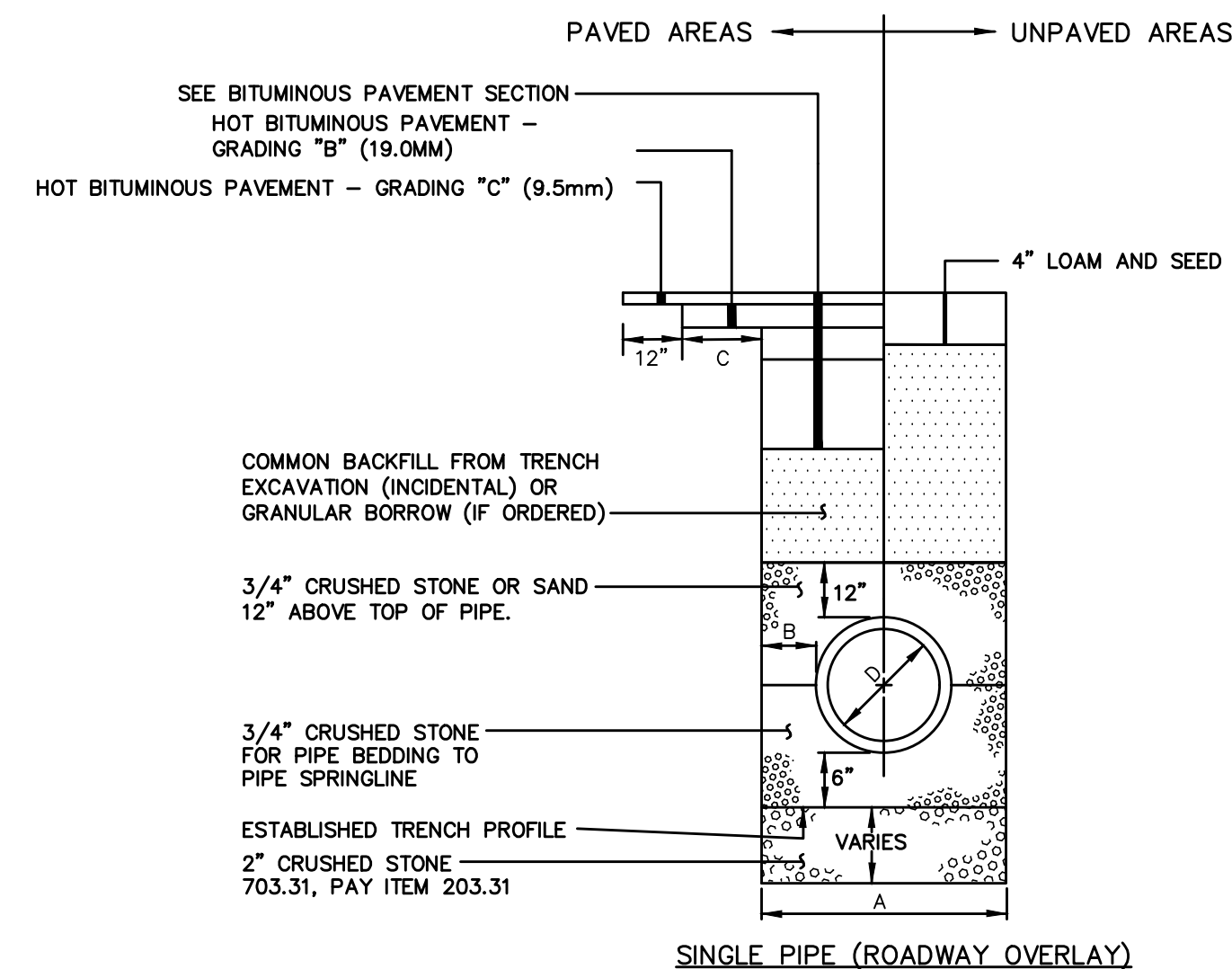
SILT FENCE	
SILT FENCE REINFORCEMENT	MAXIMUM SPACING "A"
NONE	6"
WIRE REINFORCEMENT 14 GAUGE, 6" MESH	10'



**SILTATION FENCE**

NOT TO SCALE

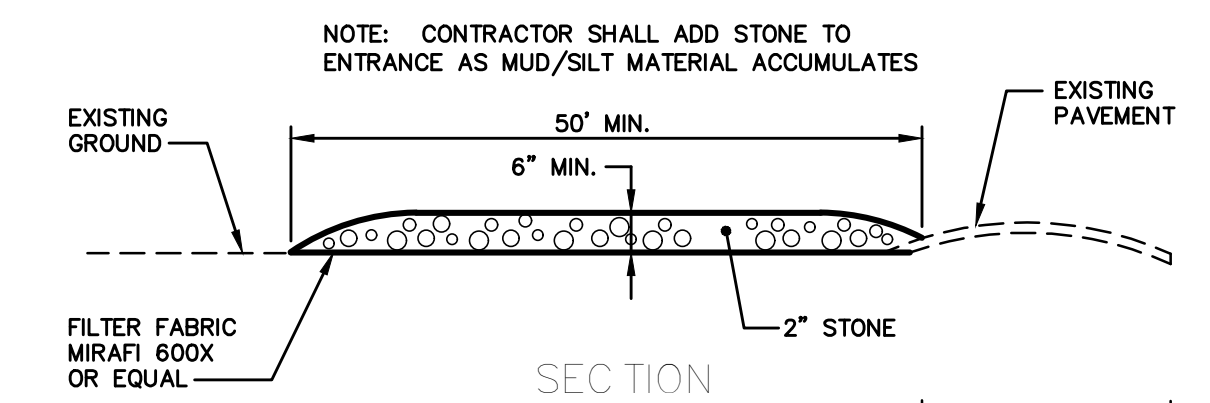
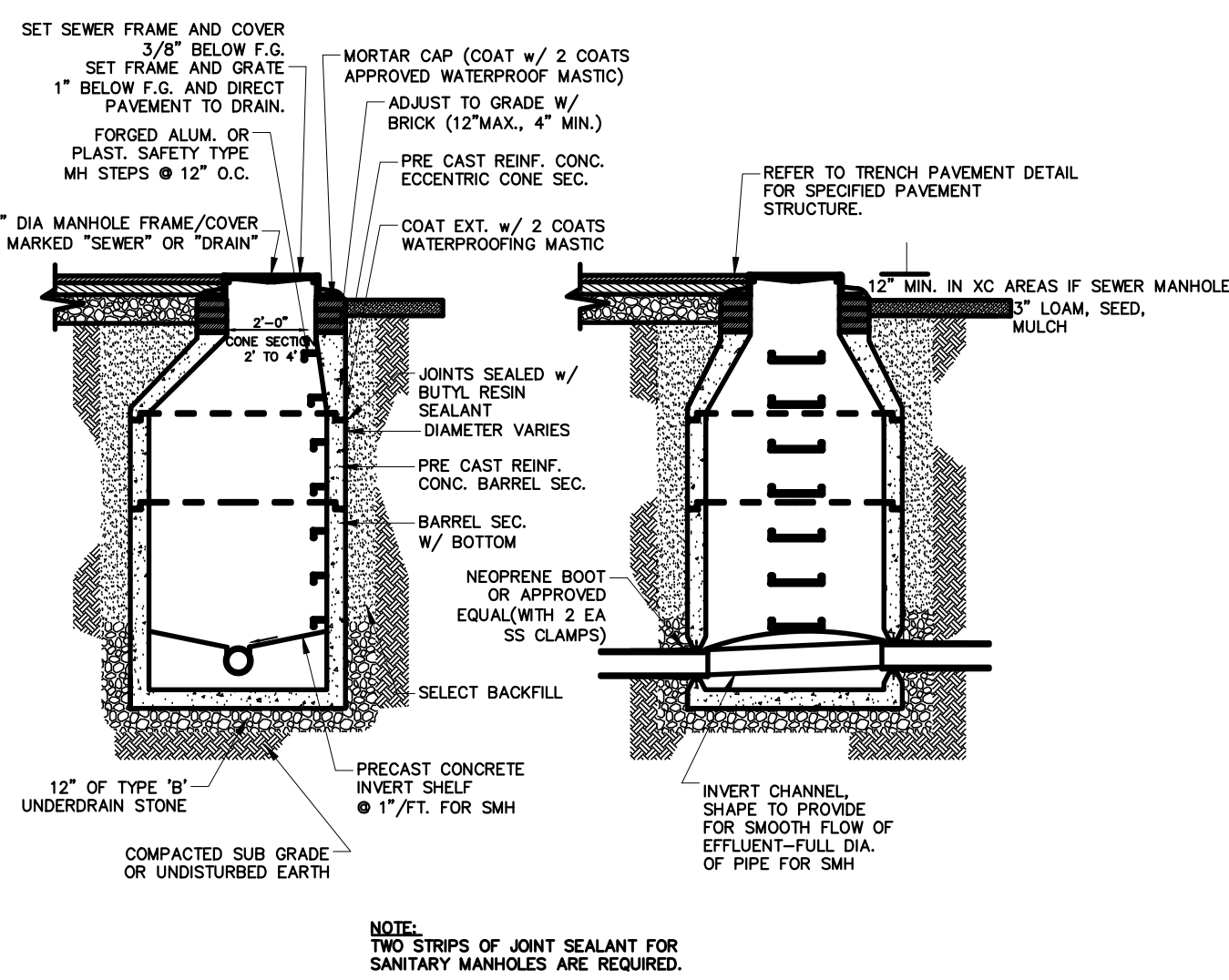
**NOTES:**  
 DEPTH OF BITUMINOUS PAVEMENT AND AGGREGATE COURSES SHALL BE DETERMINED BY THE TYPICAL ROAD SECTION.  
 ANY ALTERNATE TRENCHING OR PAYMENT METHODS SHALL BE APPROVED IN ADVANCE BY THE TOWN OF STANDISH.



**NOTES:**

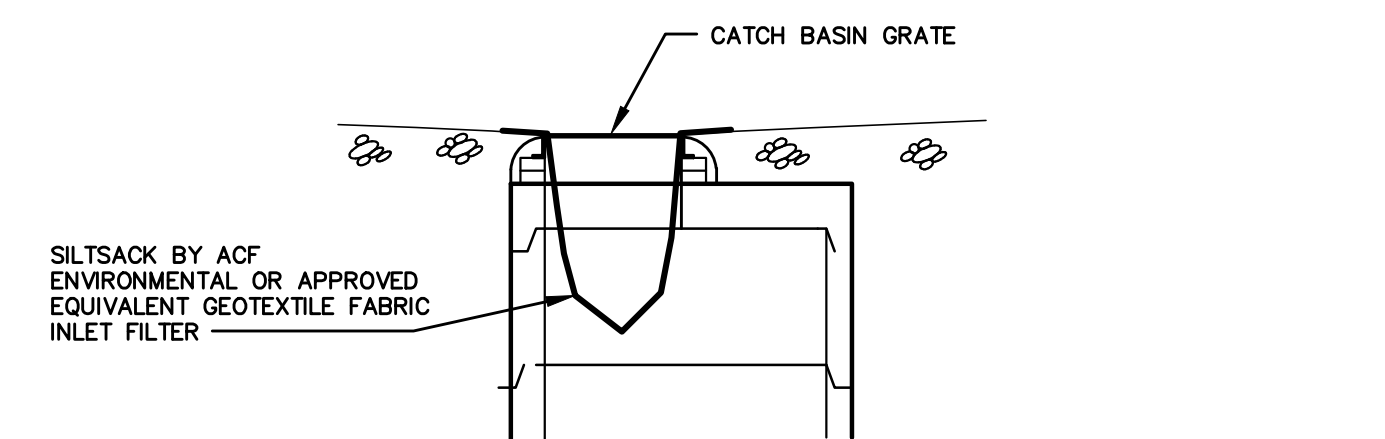
- ALTERNATIVE CONSTRUCTION METHODS OR PAYMENT METHODS SHALL BE APPROVED IN ADVANCE BY THE TOWN.
- IN PAVED AREAS, REFER TO BITUMINOUS PAVEMENT SECTION FOR DEPTHS OF GRAVEL AND HOT MIX ASPHALT PAVEMENT.
- DIMENSION "B" SHALL BE SUFFICIENT TO ALLOW CRUSHED STONE BEDDING TO BE PLACED AND COMPACTED UNDER THE HAUNCHES OF THE PIPE; BUT IN ALL CASES "B" SHALL BE AT LEAST 9".
- DIMENSION "A" IS THE MAXIMUM WIDTH ALLOWED FOR CALCULATING PAY QUANTITIES UNDER GRANULAR BORROW, CRUSHED STONE, STRUCTURAL EARTH EXCAVATION, AND STRUCTURAL ROCK EXCAVATION. DIMENSION "A" SHALL BE BASED ON PIPE DIAMETER "D", AS SET FORTH IN THE FOLLOWING TABLE.
- EXCAVATION BELOW ESTABLISHED TRENCH PROFILE (IF ORDERED). PAY ITEM 206.061.
- EXCAVATION INCIDENTAL TO PIPE PAY ITEMS (PAVED AND SEEDED AREAS)
- THE INTENT IS THE PAVE 5" OF 19MM HMA BASE IN SUMMER 2025. IN SPRING 2026, THE TRENCHES SHALL BE MILLED 1.5" TO PLACE 9.5MM HMA SURFACE PER THE TYPICAL ROAD SECTION.

PIPE DIAMETER, "D" (INCHES)	MAX. TRENCH WIDTH, "A" (FEET)	PAVEMENT WIDTH OUTSIDE TRENCH, "C" (FEET)
4	4.0	2.5
6	4.0	2.5
8	4.0	2.5
10	4.0	2.5
12	5.0	2.5
15	5.0	2.5
18	5.0	2.0
21	5.0	2.0
24	6.0	1.75
27	6.0	1.5
30	6.0	1.5
36	6.0	1.0
42	7.0	1.0
48	7.0	1.0



**STABILIZED CONSTRUCTION ENTRANCE**

NOT TO SCALE



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Rev.	Date	Revision

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Design: HWH	Draft: LAN	Date: OCT 2023
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File Name: 3863-DET.dwg		
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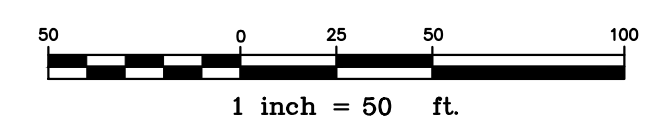
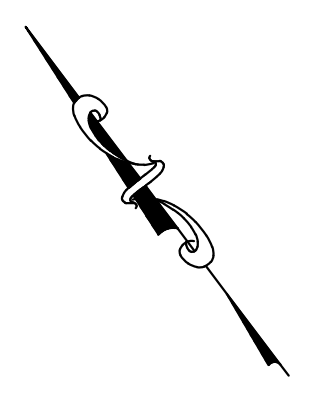
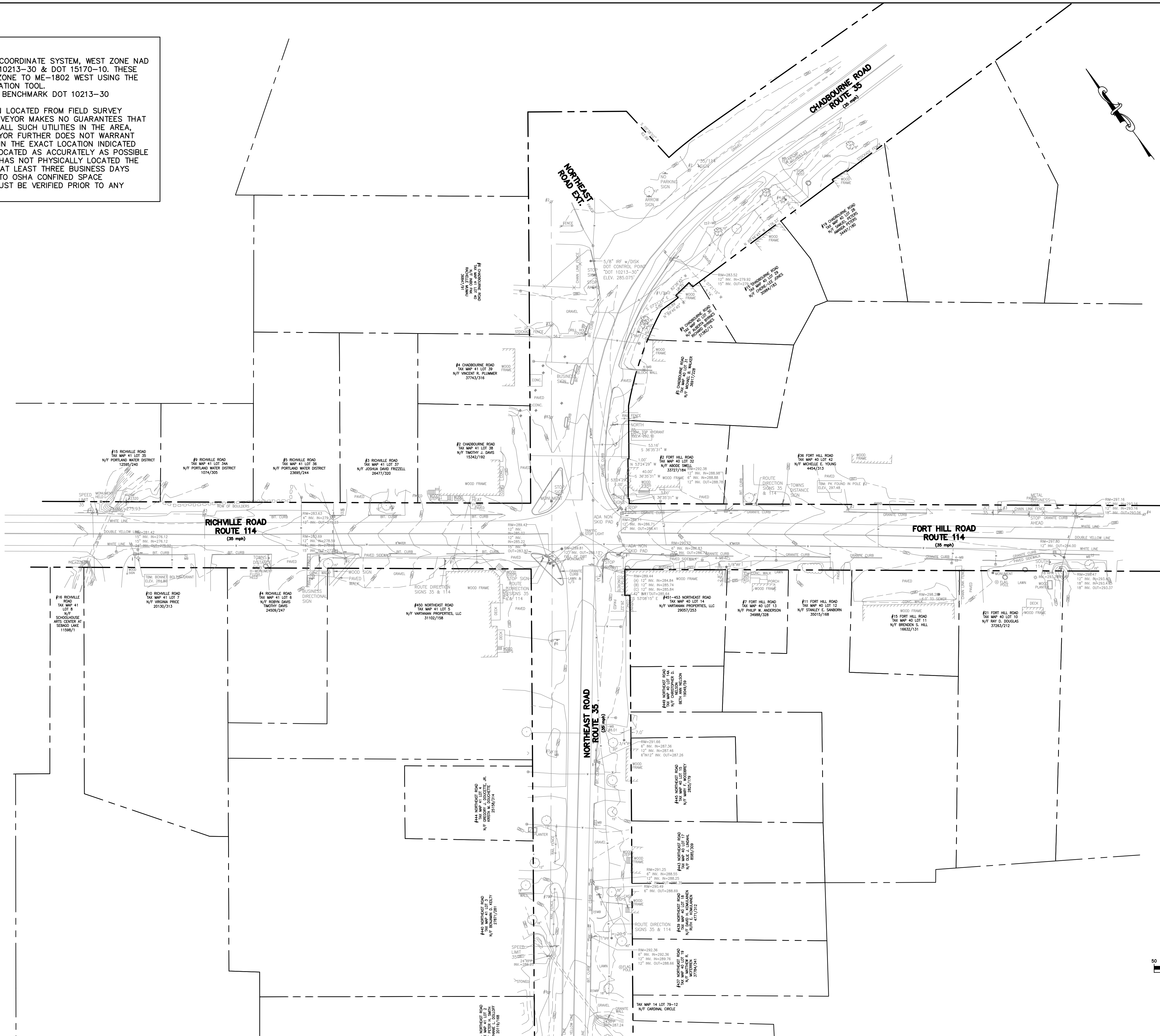
Drawing Name:	<b>MISCELLANEOUS DETAILS</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**5**



NOTES:

- BEARINGS ARE BASED ON MAINE STATE PLANE COORDINATE SYSTEM, WEST ZONE NAD 83 UTILIZING MAINE DOT CONTROL POINTS DOT 10213-30 & DOT 15170-10. THESE POINTS WERE CONVERTED FROM ME2000 WEST ZONE TO ME-1802 WEST USING THE NGS COORDINATE CONVERSION AND TRANSFORMATION TOOL.
- ELEVATIONS ARE BASED ON NAVD 88. PROJECT BENCHMARK DOT 10213-30 ELEVATION 285.075 FEET.
- THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. CALL 1-888-DIGSAFE AT LEAST THREE BUSINESS DAYS BEFORE PERFORMING AND CONSTRUCTION. DUE TO OSHA CONFINED SPACE REQUIREMENTS, ALL INVERTS AND PIPE SIZES MUST BE VERIFIED PRIOR TO ANY CONSTRUCTION.



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Rev.	Date	Revision

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Design: HWH Draft: LAN Date: OCT 2023  
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Drawing Name: **EXISTING CONDITIONS PLAN**  
 Project: **Layout & Signalization of Route 114 & 35 Intersection**  
 Standish, Maine  
 Client: **Town of Standish**  
 175 Northeast Road, Standish, Maine 04084

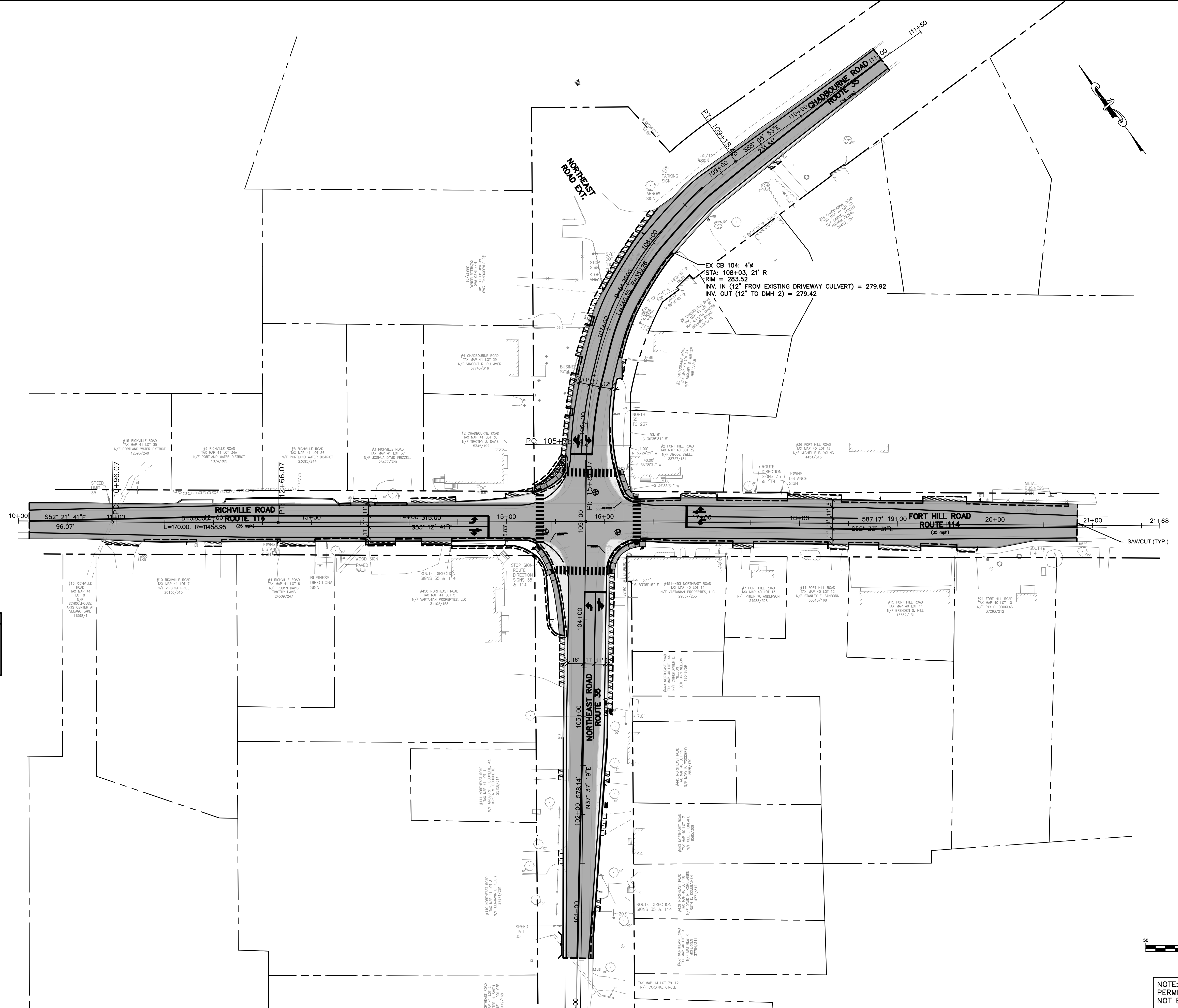
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6







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PAVEMENT LEGEND	
	STRIP AND REPAVE
	ROAD WIDENING

Rev.	Date	Revision

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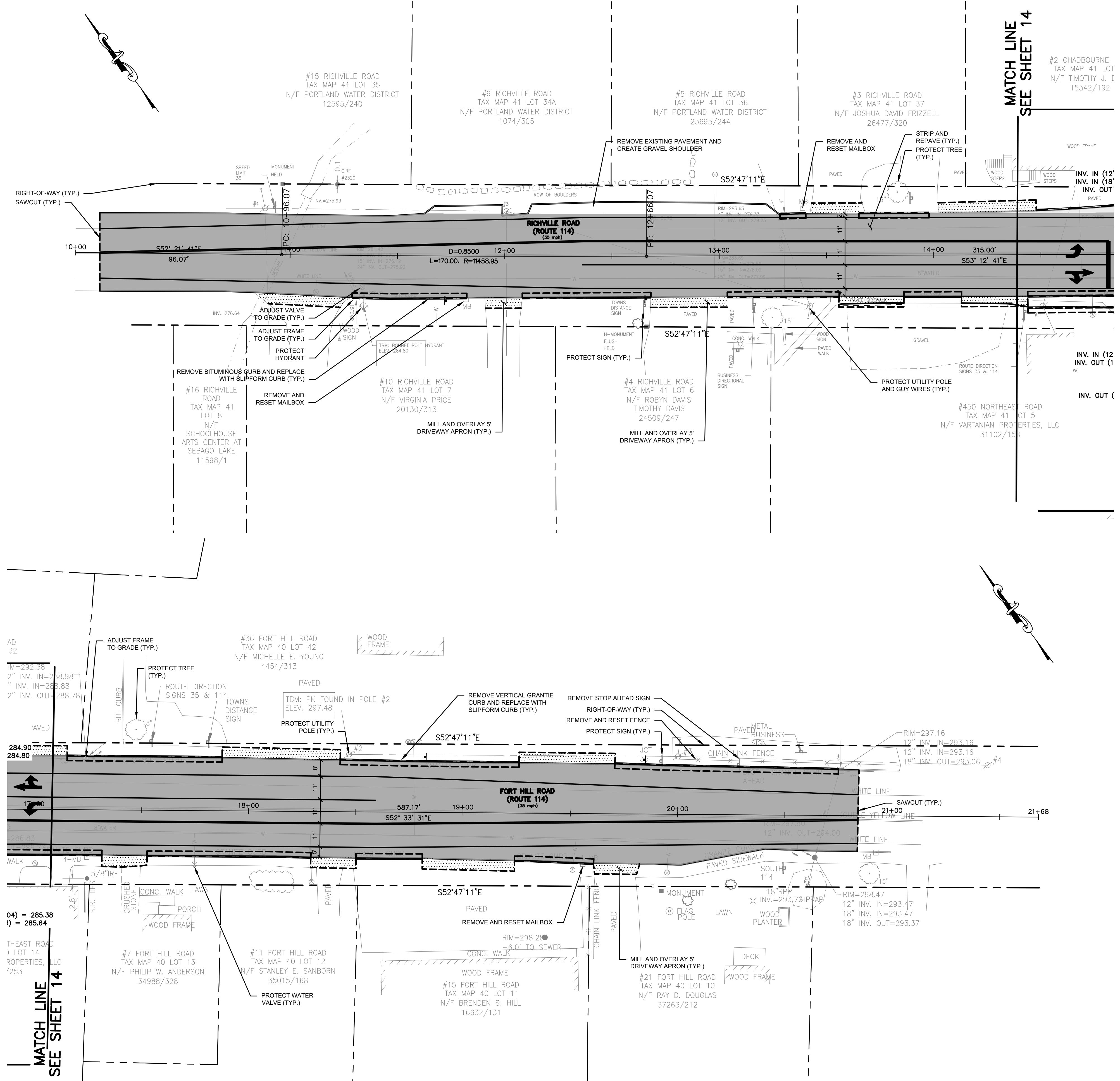


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Drawing Name:	<b>OVERALL PLAN</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**8**

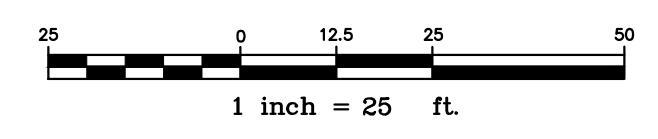




PAVEMENT LEGEND	
	STRIP AND REPAVE
	ROAD WIDENING

CURBING LEGEND	
	SLIPFORM CURB TYPE 3, MOLD 3
	SLIPFORM CURB TYPE 3, MOLD 2



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Rev.	Date	Revision

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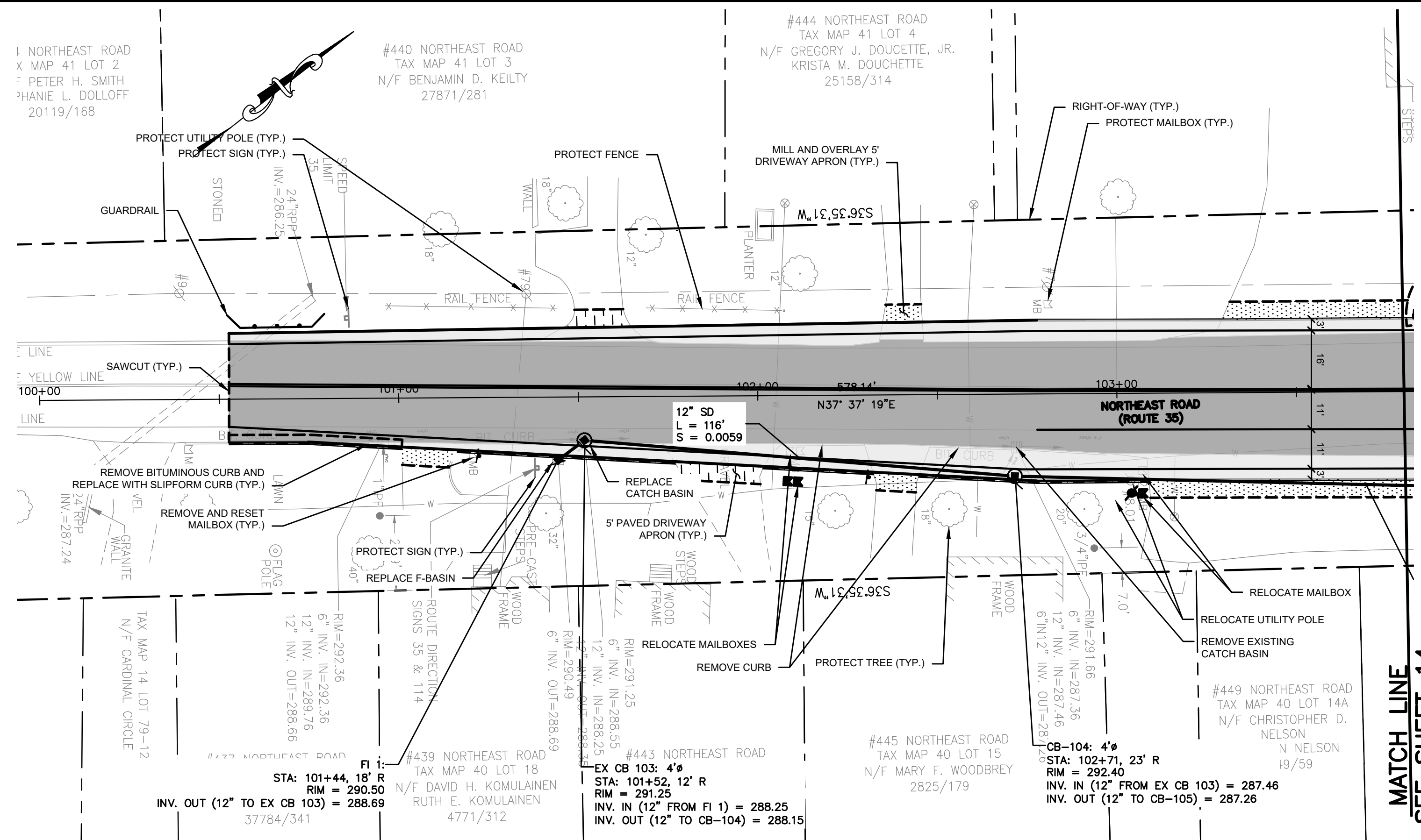
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Drawing Name: **GENERAL PLAN - ROUTE 114**  
 Project: **Layout & Signalization of Route 114 & 35 Intersection**  
 Standish, Maine  
 Client: **Town of Standish**  
 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**9**



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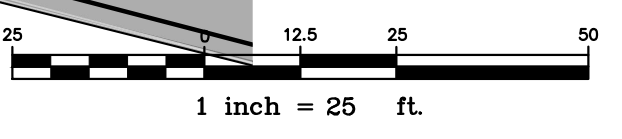
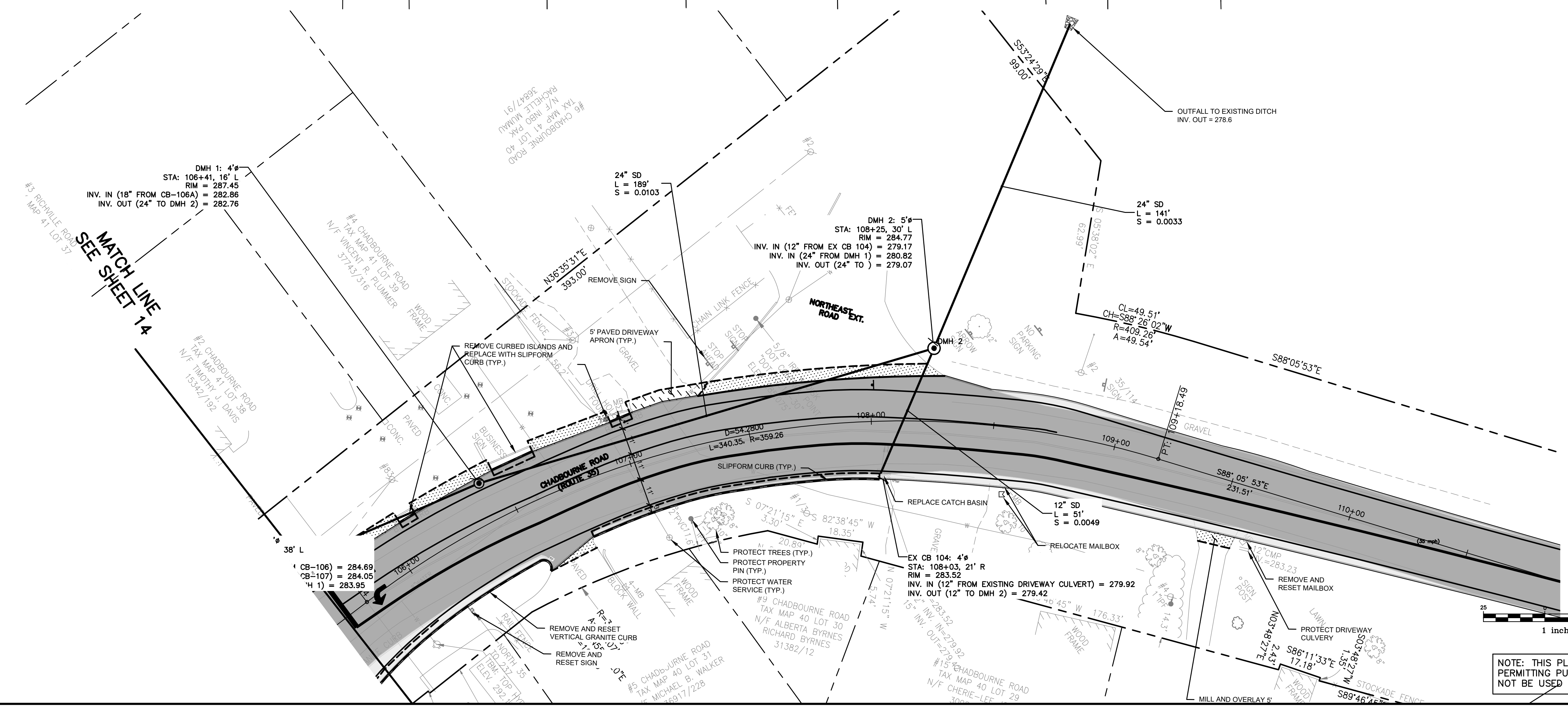


MATCH LINE 14  
SEE SHEET 14

PAVEMENT LEGEND	
	STRIP AND REPAVE
	ROAD WIDENING

CURBING LEGEND	
	CURB TYPE 3, MOLD 3
	CURB TYPE 3, MOLD 2



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Rev.	Date	Revision

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Design: HWH Draft: LAN Date: OCT 2023  
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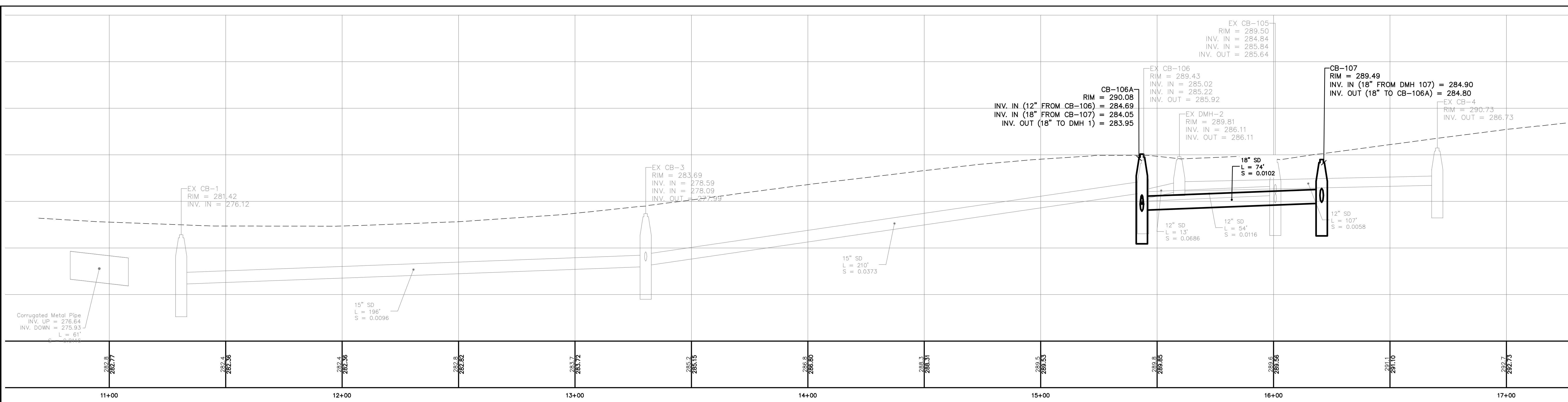
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Drawing Name: **GENERAL PLAN - ROUTE 35**  
 Project: **Layout & Signalization of Route 114 & 35 Intersection**  
 Standish, Maine  
 Client: **Town of Standish**  
 175 Northeast Road, Standish, Maine 04084

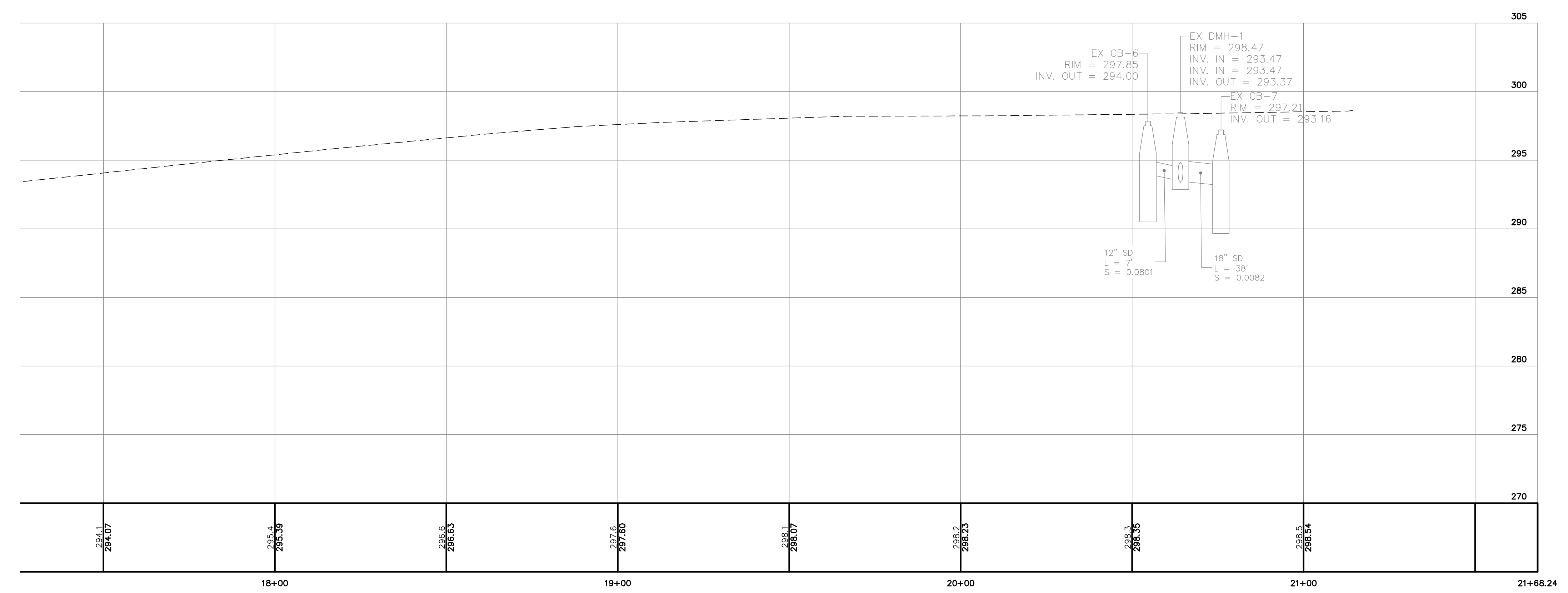
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**10**



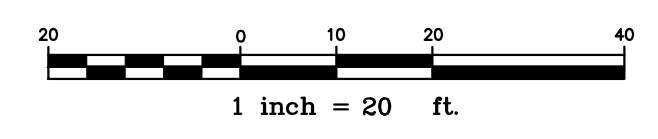
U:\3863\_Standish\_Rte 114 & 35 Adaptive Signal\1 - CAD\DWG\3863-PBASE.dwg 11/2/2023 2:49 PM



**ROUTE 114 PROFILE**  
SCALE: 1" = 20' HORIZ.  
1" = 5' VERT.



**ROUTE 114 PROFILE**  
SCALE: 1" = 20' HORIZ.  
1" = 5' VERT.



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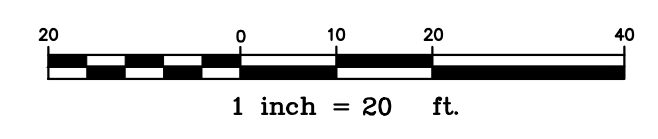
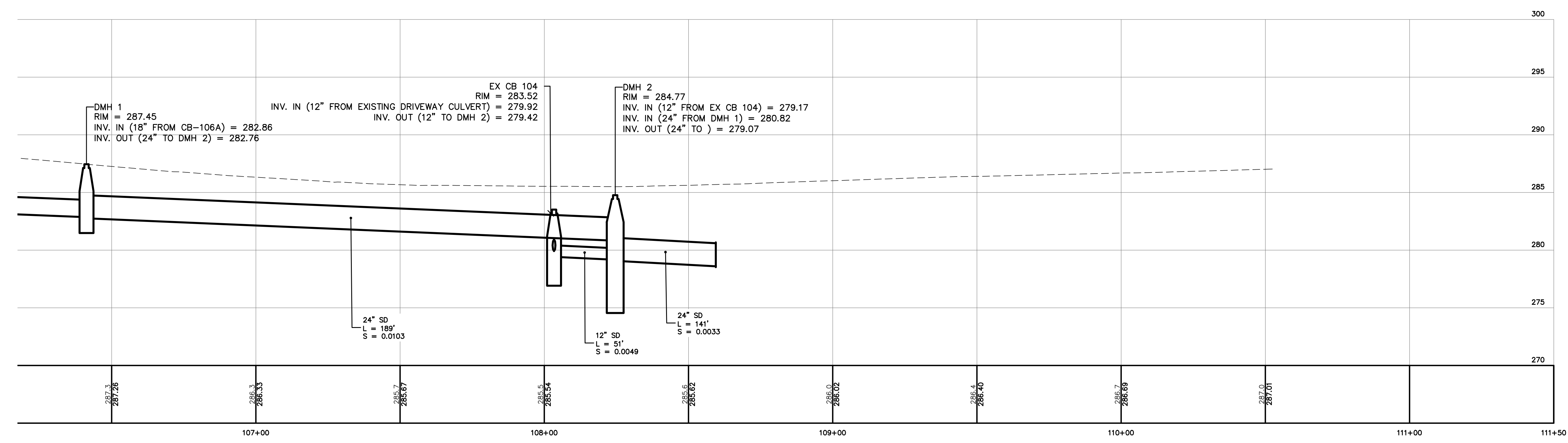
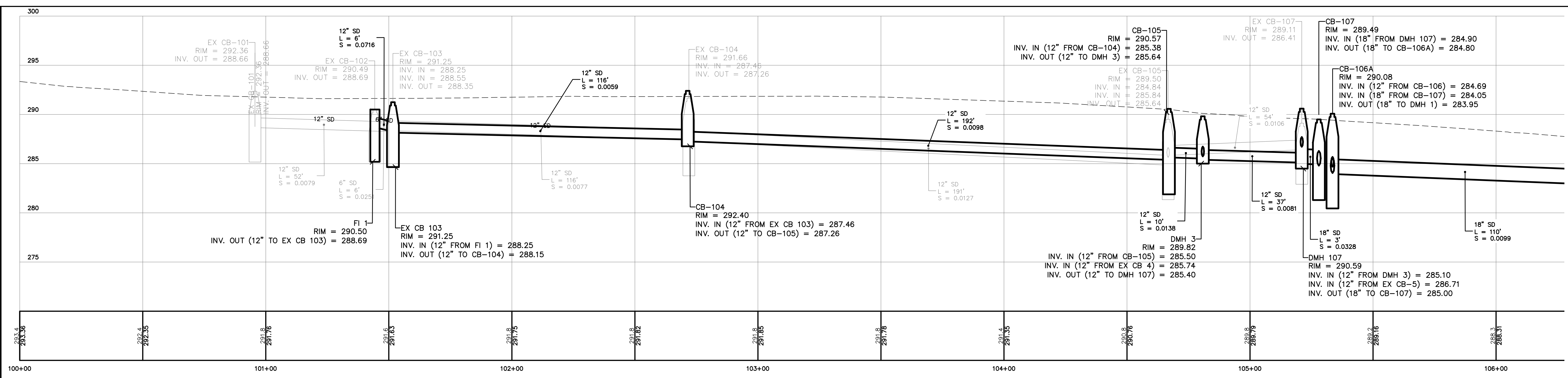
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Drawing Name: **ROUTE 114 PROFILE**  
Project: **Layout & Signalization of Route 114 & 35 Intersection**  
Standish, Maine  
Client: **Town of Standish**  
175 Northeast Road, Standish, Maine 04084

Drawing No.  
**11**



U:\3863\_Standish\_Rte 114 & 35 Adaptive Signal\3 - CAD\DWG\3863-PBASE.dwg 11/2/2023 2:49 PM



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Rev.	Date	Revision

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Design:	HWH	Draft:	LAN	Date:	OCT 2023
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File Name:	3863-PBASE.dwg				

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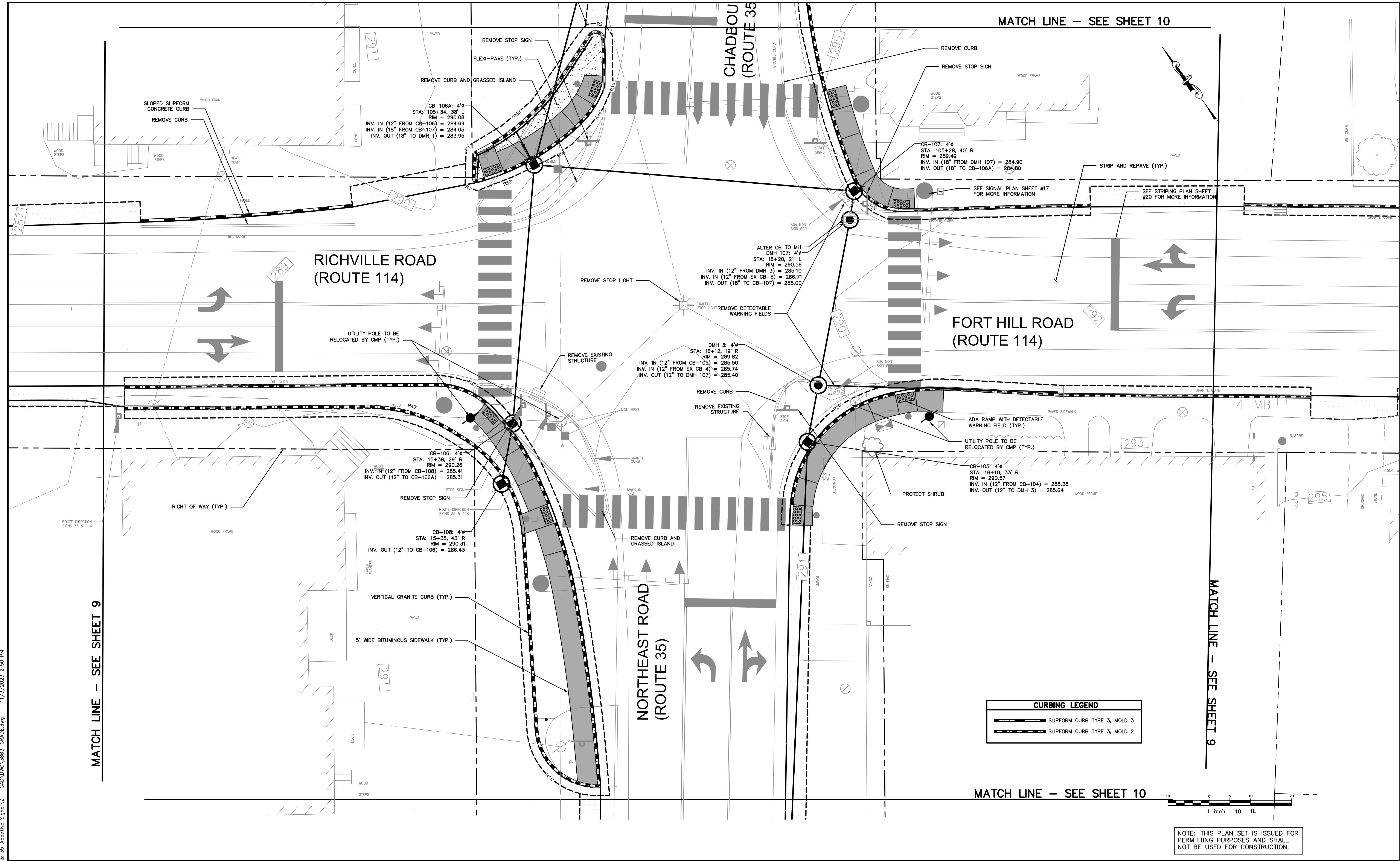
Drawing Name:	<b>ROUTE 35 PROFILE</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**12**









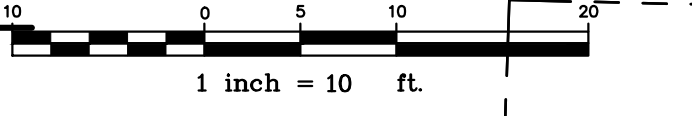
MATCH LINE - SEE SHEET 9

MATCH LINE - SEE SHEET 10

MATCH LINE - SEE SHEET 9

MATCH LINE - SEE SHEET 10

CURBING LEGEND	
	SLIPPFORM CURB TYPE 3, MOLD 3
	SLIPPFORM CURB TYPE 3, MOLD 2



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Issued For	Date	By

Design:	HWH	Draft:	LAN	Date:	OCT 2023
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File Name:	3863-GRADE.dwg				
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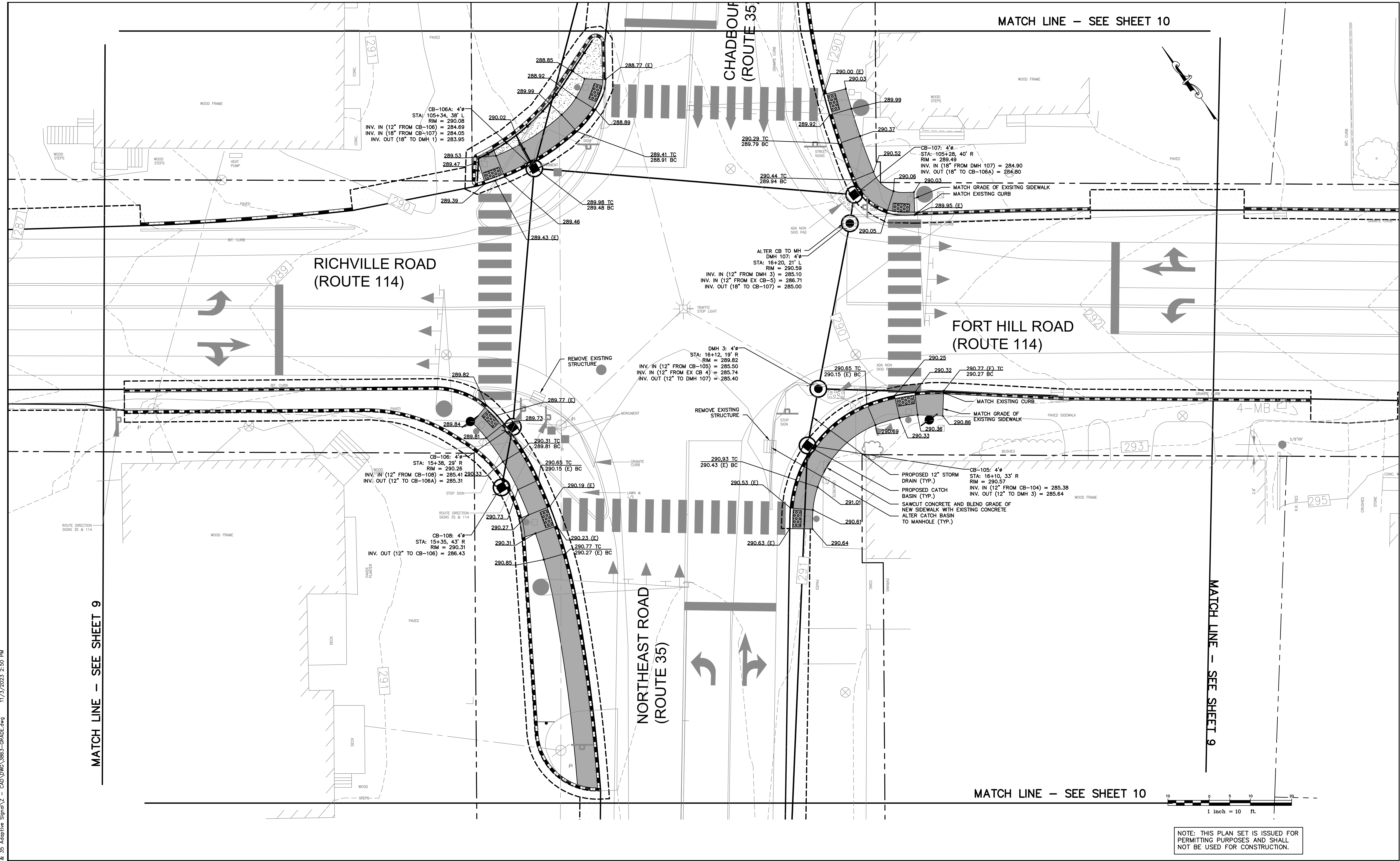
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Drawing Name:	<b>INTERSECTION LAYOUT PLAN</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**14**



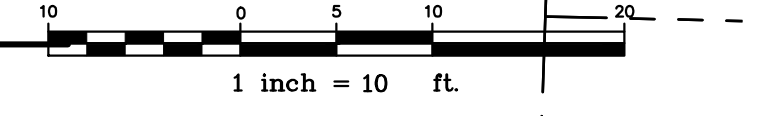


MATCH LINE - SEE SHEET 10

MATCH LINE - SEE SHEET 9

MATCH LINE - SEE SHEET 9

MATCH LINE - SEE SHEET 10



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Rev.	Date	Revision

Issued For	Date	By

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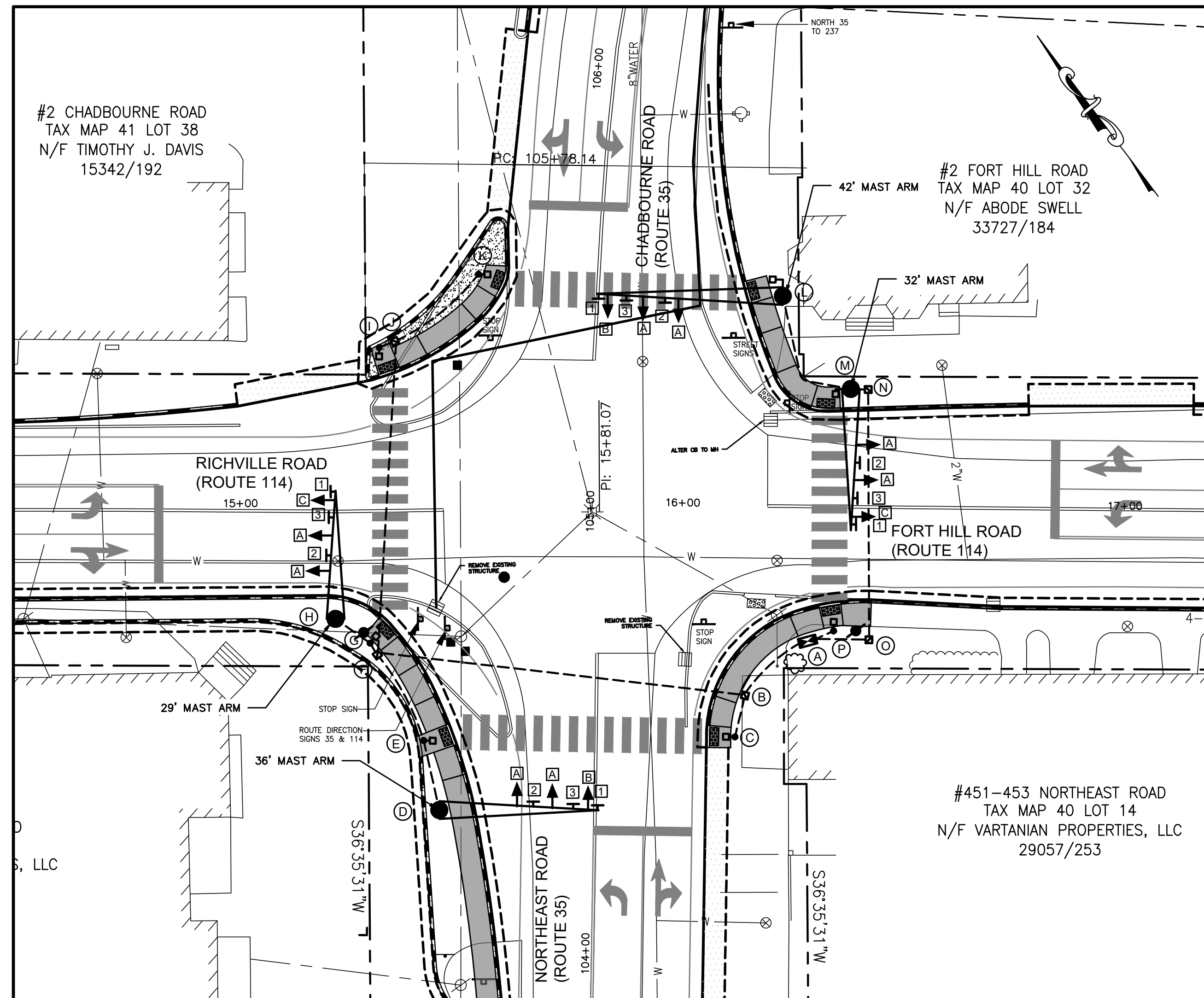
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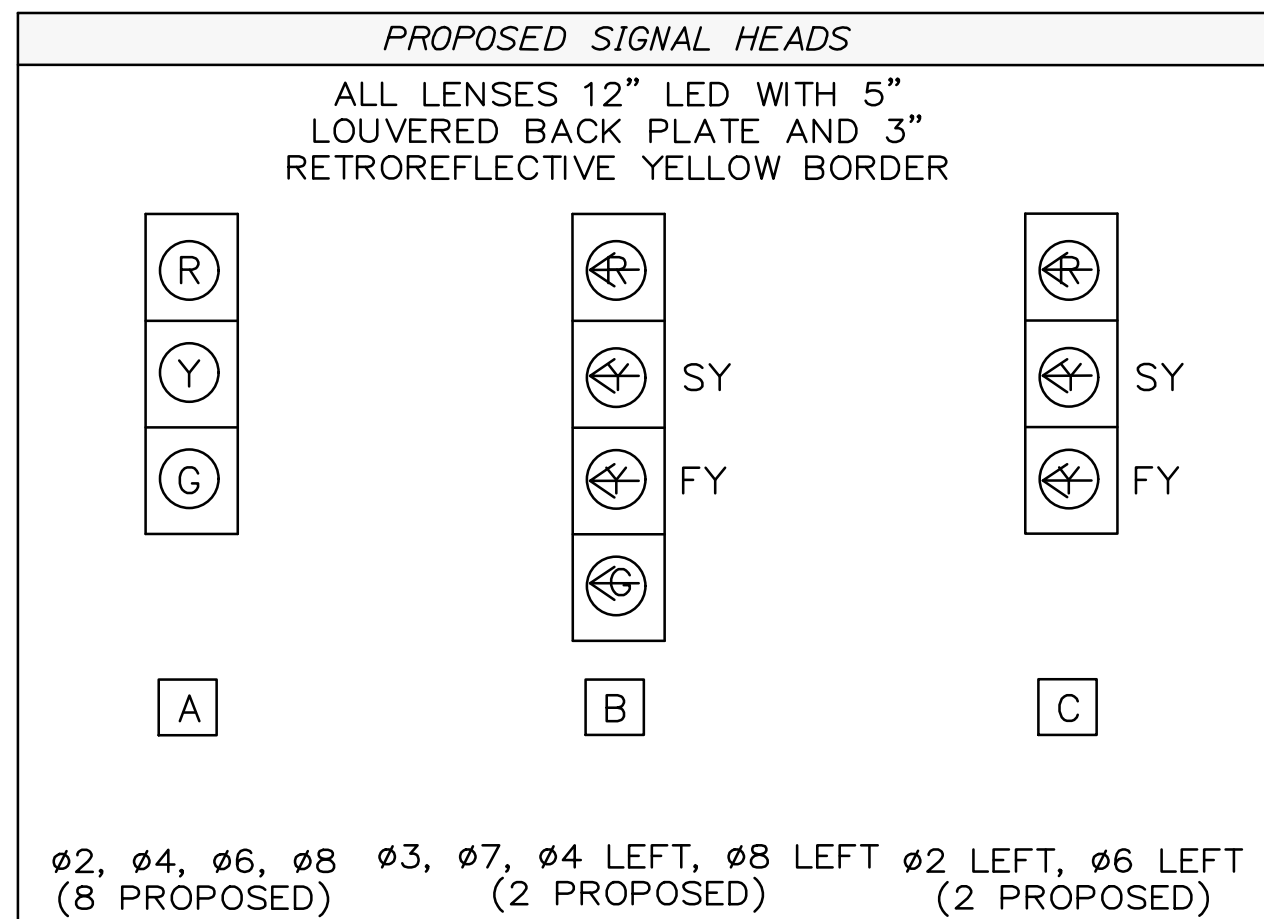
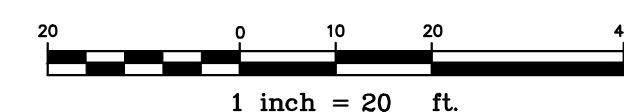
Drawing Name:	<b>INTERSECTION GRADING PLAN</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**15**



**SIGNAL PLAN**

SCALE: 1" = 20'



Ø2, Ø4, Ø6, Ø8 Ø3, Ø7, Ø4 LEFT, Ø8 LEFT Ø2 LEFT, Ø6 LEFT  
(8 PROPOSED) (2 PROPOSED) (2 PROPOSED)

- SIGNAL HEADS TO BE ALIGNED PER MUTCD STANDARDS
- SY=SOLID YELLOW
- FY=FLASHING YELLOW

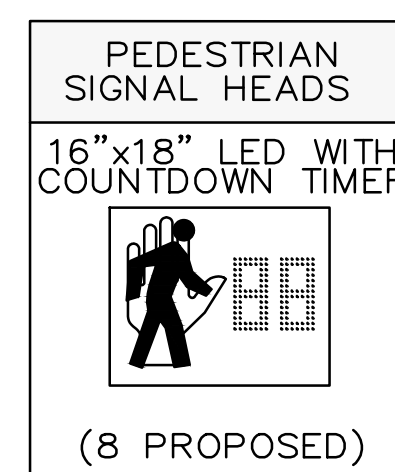
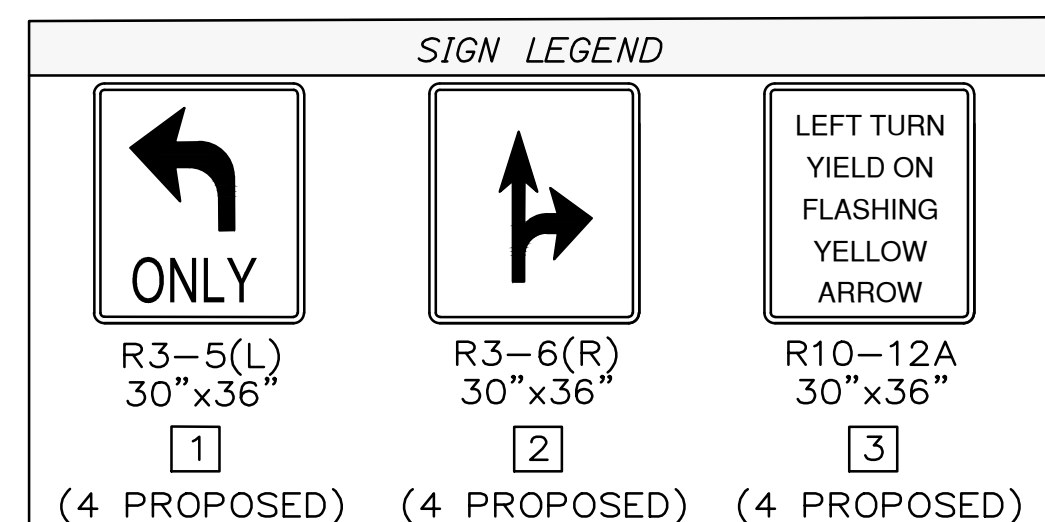
**LEGEND**

EXISTING	SIGNAL NOTE	PROPOSED
→	SIGNAL HEAD	⊗
○	DILEMMA ZONE DETECTOR	○
□	VEHICLE DETECTION	■
⬅	EMERGENCY PRE-EXEMPTION RECEIVER	⬅
⬆	EMERGENCY PRE-EXEMPTION STROBE	⬆
---	SIGNAL CONDUIT (3" PVC, SCH 80)	---
---	METALLIC CONDUIT	---
⊠	CONTROLLER CABINET	⊠
⊠	PEDESTRIAN SIGNAL HEAD	⊠
○	PEDESTAL POLE	○
○	UTILITY POLE	○
⊠	MAST ARM WITH SIGNAL STANDARD	⊠
⊠	OVERHEAD ILLUMINATION	⊠
⊠	MOUNTED SIGN	⊠
⊠	PULL BOX	⊠
⊠	DETECTABLE WARNING PANEL	⊠
	PROPOSED OR REPAINTED CROSSWALK	

**EQUIPMENT SCHEDULE**

DESIGNATION	ITEM	DESCRIPTION	LOCATION
A	NEW ATC 4 DOOR RACK MOUNT CONTROLLER	GROUND MOUNTED WITH BASE EXTENSION, CONCRETE FOUNDATION AND NEW CONTROLLER	16+28.38, 29.05' RT
B, F, J, N, O,	PRECAST CONCRETE PULL BOX	INSTALL PULL BOX	(16+14.3, 41.44' RT) (15+30.7, 32.30' RT) (15+34.9, 38.58' LT) (16+41.6, 27.97' LT) (16+42.2, 28.63' RT)
C	DUAL PURPOSE PEDESTAL POLE **	INSTALL 12' PEDESTAL POLE, OVERHEAD ILLUMINATION, PEDESTRIAN COUNTDOWN HEAD, PEDESTRIAN SIGN, AND APS PUSH BUTTON ON NEW 24" CONCRETE FOUNDATION	16+12.1, 50.82' RT
D	SIGNAL POLE WITH 36' MAST ARM	SIGNAL HEADS, VEHICLE DETECTION AND EMERGENCY PRE-EMPTION AND CONFIRMATION STROBE, ON NEW CONCRETE FOUNDATION (DESIGNED IN ACCORDANCE WITH MAINE DOT STANDARD SPECIFICATION 626.034)	15+44.6, 67.47' RT
E	DUAL PURPOSE PEDESTAL POLE **	INSTALL 12' PEDESTAL POLE, OVERHEAD ILLUMINATION, PEDESTRIAN COUNTDOWN HEAD, PEDESTRIAN SIGN, AND APS PUSH BUTTON ON NEW 24" CONCRETE FOUNDATION	15+41.2, 51.73' RT
G	DUAL PURPOSE PEDESTAL POLE **	INSTALL 12' PEDESTAL POLE, OVERHEAD ILLUMINATION, PEDESTRIAN COUNTDOWN HEAD, PEDESTRIAN SIGN, AND APS PUSH BUTTON ON NEW 24" CONCRETE FOUNDATION	15+29.1, 29.66' RT
H	SIGNAL POLE WITH 29' MAST ARM	SIGNAL HEADS, VEHICLE DETECTION AND EMERGENCY PRE-EMPTION AND CONFIRMATION STROBE, ON NEW CONCRETE FOUNDATION (DESIGNED IN ACCORDANCE WITH MAINE DOT STANDARD SPECIFICATION 626.034)	15+21.3, 24.15' RT
I	DUAL PURPOSE PEDESTAL POLE **	INSTALL 12' PEDESTAL POLE, OVERHEAD ILLUMINATION, PEDESTRIAN COUNTDOWN HEAD, PEDESTRIAN SIGN, AND APS PUSH BUTTON ON NEW 24" CONCRETE FOUNDATION	15+31.4, 37.2' LT
K	DUAL PURPOSE PEDESTAL POLE **	INSTALL 12' PEDESTAL POLE, OVERHEAD ILLUMINATION, PEDESTRIAN COUNTDOWN HEAD, PEDESTRIAN SIGN, AND APS PUSH BUTTON ON NEW 24" CONCRETE FOUNDATION	15+53.9, 53.67' LT
L	SIGNAL POLE WITH 42' MAST ARM	SIGNAL HEADS, VEHICLE DETECTION AND EMERGENCY PRE-EMPTION AND CONFIRMATION STROBE, ON NEW CONCRETE FOUNDATION (DESIGNED IN ACCORDANCE WITH MAINE DOT STANDARD SPECIFICATION 626.034)	16+22.2, 48.99' LT
M	SIGNAL POLE WITH 32' MAST ARM	SIGNAL HEADS, VEHICLE DETECTION AND EMERGENCY PRE-EMPTION AND CONFIRMATION STROBE, ON NEW CONCRETE FOUNDATION (DESIGNED IN ACCORDANCE WITH MAINE DOT STANDARD SPECIFICATION 626.034)	16+37.7, 28.09' LT
P	DUAL PURPOSE PEDESTAL POLE **	INSTALL 12' PEDESTAL POLE, OVERHEAD ILLUMINATION, PEDESTRIAN COUNTDOWN HEAD, PEDESTRIAN SIGN, AND APS PUSH BUTTON ON NEW 24" CONCRETE FOUNDATION	16+34.2, 26.68 RT

- \* EQUIPMENT TO BE ADJUSTED AS NEEDED BY RESIDENT, MAINEDOT, OR CITY AS DIRECTED
- \*\* OVERHEAD ILLUMINATION SHALL BE DIMMABLE



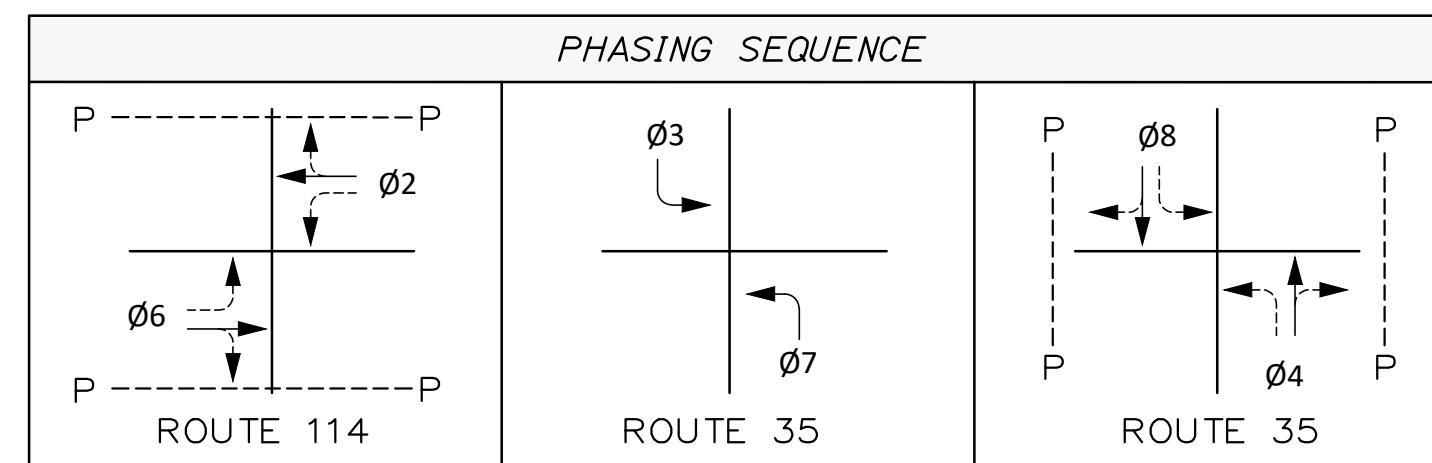
**NOTES:**

1. THESE TIMINGS ARE FOR INITIAL START UP ONLY AND SHALL BE FIELD ADJUSTED AS NEEDED UNDER CITY OR MAINEDOT DIRECTION WITHIN TWO WEEKS OF START UP.
2. PEDESTRIAN CLEARANCE MAY USE YELLOW TIME.

**SIGNAL TIMING SCHEDULE - FREE PLAN**

	1	2	3	4	5	6	7	8	9
MINIMUM INITIAL VEHICLE EXTENSION	-	10	5	5	-	10	5	5	-
MAX I	-	3	3	3	-	3	3	3	-
MAX II	-	25	10	29	-	25	10	29	-
YELLOW	-	4	3.5	4	-	4	3.5	4	-
ALL RED	-	2	1	2	-	2	1	2	-
WALK / DON'T WALK	-	5	-	5	-	5	-	5	-
PEDESTRIAN CLEARANCE	-	16	-	16	-	16	-	16	-
FLASH	-	Y	R	R	-	Y	R	R	-
PHASE RECALL	-	SOFT	-	-	-	SOFT	-	-	-
MEMORY	-	-	-	-	-	-	-	-	-
DETECTOR	-	PR	PR	PR	-	PR	PR	PR	-

Y=YELLOW FY=FLASH YELLOW ARROW R=RED PR=PRESENCE



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File Name: 3863-SIGNAL.dwg		
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Drawing Name: **SIGNAL PLAN**

Project: **Layout & Signalization of Route 114 & 35 Intersection**  
Standish, Maine

Client: **Town of Standish**  
175 Northeast Road, Standish, Maine 04084

Drawing No. **16**



**SIGNAL NOTES**

1. ALL MATERIALS AND WORK SHALL CONFORM TO THE TOWN OF STANDISH AND MAINE DOT STANDARD SPECIFICATIONS AND BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", U.S.D.O.T., F.H.W.A., LATEST EDITION.
2. ALL SIGNAL AND SIGNAL RELATED EQUIPMENT SHALL MEET OR EXCEED THE TOWN OF STANDISH TRAFFIC SIGNAL STANDARDS.
3. IT IS THE INTENT OF THIS WORK TO HAVE A COMPLETE OPERATIONAL, TESTED AND ACCEPTED TRAFFIC SIGNAL UPON COMPLETION OF THIS CONTRACT.
4. TRAFFIC SIGNAL WORK SHALL BE COMPLETED IN A MANNER AND ORDER THAT WILL CAUSE THE MINIMUM DISRUPTION TO TRAFFIC.
5. THE RESIDENT, TOWN AND MAINE DOT SHALL HAVE THE RIGHT AND AUTHORITY TO DETERMINE THE ACCEPTABILITY OF WORK AND MATERIALS IN PROGRESS OR COMPLETED AND SHALL HAVE THE RIGHT TO REJECT ANY WORK OR MATERIALS WHICH DO NOT CONFORM, IN ITS SOLE OPINION, TO THE PLANS OR SPECIFICATIONS.
6. THE LOCATIONS OF SIGNAL EQUIPMENT AND POLES INCLUDING THE MOUNTED TRAFFIC DEVICES AND SIGNS ARE APPROXIMATE. FINAL LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT, MAINE DOT OR TOWN REPRESENTATIVE.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY TRAFFIC SIGNALS AND ASSOCIATED WORK IF REQUIRED. CONTRACTOR SHALL REMOVE THE TEMPORARY TRAFFIC SIGNAL ONCE NEW SIGNAL IS OPERATIONAL. ALL COSTS ASSOCIATED WITH TEMPORARY SIGNALS SHALL BE INCIDENTAL TO THE 643 ITEMS.
8. TWO COPIES OF AS-BUILT PLANS, SIGNAL TIMING, WIRING DIAGRAMS, BOX PRINTS AND EQUIPMENT MANUALS SHALL BE LEFT IN THE CONTROLLER CABINET. ONE ELECTRONIC COPY OF EACH SHALL BE PROVIDED TO THE TOWN PLANNER AND PUBLIC WORKS DIRECTOR.
9. CONTRACTOR SHALL PROVIDE AND INSTALL ALL PULL BOXES PER MAINE DOT STANDARDS AT A MINIMUM.
10. THE CONTRACTOR SHALL PREPARE A MATERIAL SCHEDULE BASED UPON THEIR PLAN REVIEW. ALL SCHEDULES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS OR PERFORMING WORK.
11. UTILITIES THAT HAVE FACILITIES IN THE GENERAL PROJECT ARE LISTED IN SPECIAL PROVISION 104.
12. UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY AND THE CONTRACTOR IS RESPONSIBLE FOR FINDING EXACT LOCATIONS OF UTILITIES PRIOR TO CONSTRUCTION. CONTRACTOR SHALL CONTACT DIG SAFE AT 1-888-DIG-SAFE, CHECK 'OKTODIG.COM', AND CONTACT APPROPRIATE AUTHORITIES PRIOR TO ANY SUBSURFACE ACTIVITIES.
13. THE CONTRACTOR SHALL MEET ALL REQUIREMENTS OF THE UTILITY COMPANIES WHEN MODIFYING THE EXISTING SERVICE CONNECTIONS AND WHEN INSTALLING EQUIPMENT ON THEIR POLES OR NEAR THEIR WIRES.
14. THE CONTRACTOR IS RESPONSIBLE FOR THE RELOCATION AND/OR INSTALLATION OF POWER METERS IF REQUIRED AND ASSURING THE POWER METER LOCATION AND INSTALLATION CONFORMS TO THE LOCAL UTILITY REQUIREMENTS. THIS WORK WILL BE INCIDENTAL TO ITEM 643.71.
15. ALL CONDUIT CROSSING AREAS OF EXISTING PAVEMENT SHALL BE INSTALLED USING TRENCHLESS TECHNOLOGIES, UNLESS OTHERWISE APPROVED.
16. ALL NEW SIGNAL SECTIONS SHALL HAVE LED LENSES 12 INCHES IN DIAMETER, AND SHALL HAVE 5 INCH LOUVERED BACK PLATES WITH 3" YELLOW RETROREFLECTIVE STRIP AROUND BORDER OF BACK PLATES.
17. ALL SPLICES WILL BE MADE IN THE CABINETS OR POLES MEETING MAINE DOT SPECIFICATIONS.
18. THE CONTRACTOR SHALL PROVIDE MAST ARM DESIGN IN ACCORDANCE WITH SECTION 643.023.
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINAL STRUCTURAL DESIGN OF THE SIGNAL SUPPORT STRUCTURES AND THE CONNECTION OF THE SUPPORT STRUCTURES TO THEIR FOUNDATIONS. ALL DESIGNS SHALL BE PREPARED AND STAMPED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF MAINE. DESIGN COMPUTATIONS, INCLUDING DESIGN LOADS (OVERTURNING MOMENT, TORSION, SHEAR FORCE, AND AXIAL LOAD) AT THE TOP OF THE FOUNDATIONS, AND SHOP DRAWINGS SHALL BE SUBMITTED FOR APPROVAL BY THE DEPARTMENT. NO MATERIALS SHALL BE ORDERED, FABRICATED, OR INSTALLED UNTIL THE DESIGN HAS BEEN APPROVED.
20. SIGNAL AND SIGN MAST ARM FOUNDATIONS SHALL CONSIST OF CAST-IN-PLACE REINFORCED CONCRETE DRILLED SHAFTS. ONE NEW DRILLED SHAFT PER MAST ARM AS SET FORTH IN STANDARD SPECIFICATIONS 643, 645 AND 626. PRELIMINARY FOUNDATION SIZES ARE PROVIDED WITH THESE PLANS. MAINE DOT WILL PROVIDE THE CONTRACTOR WITH FINAL FOUNDATION DESIGN RECOMMENDATIONS DURING CONSTRUCTION ONCE FINAL STRUCTURAL DESIGN INFORMATION AND DESIGN LOADS FOR THE SIGNAL SUPPORTS ARE PROVIDED BY THE CONTRACTOR. DRILLED SHAFTS SHALL NOT BE PERMANENTLY CASED EXCEPT FOR THE TOP 3 FEET. CONCRETE SHALL BE CAST DIRECTLY AGAINST THE SURROUNDING SOIL. PRECAST FOUNDATIONS WILL NOT BE PERMITTED.
21. ANY DAMAGE TO SLOPES, NON-BRICK SIDEWALKS OR PAVEMENT RESULTING FROM INSTALLATION OF MAST ARM POLES AND FOUNDATIONS, SHALL BE REPAIRED BY THE CONTRACTOR AS DIRECTED BY THE RESIDENT AND/OR TOWN OF STANDISH. COSTS OF REPAIRS SHALL BE INCIDENTAL TO PAYMENT UNDER SECTION 643.
22. PAYMENT FOR MAST ARM OR DUAL PURPOSE POLES AND FOUNDATIONS SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS 643, 645 AND 626 AND WILL BE FULL COMPENSATION FOR ALL LABOR, EQUIPMENT AND MATERIALS FOR THE ACCEPTED SIGNAL OR SIGN INSTALLATION INCLUDING BUT NOT LIMITED TO SIGNS, SIGNALS, SUPPORT STRUCTURES AND FOUNDATIONS, EXCAVATION, EXCAVATION STABILIZATION, BACKFILL, REPLACEMENT OF SUBBASE GRAVEL, REPLACEMENT OR REPAIR OF HIGHWAY PAVEMENT, SIDEWALK, SLOPE REGRADING, AND PLACEMENT OF LOAM, SEED AND MULCH ON DISTURBED SLOPES. PAYMENT FOR CONSTRUCTION TRAFFIC CONTROL WILL BE MADE UNDER APPLICABLE PAY ITEMS OF SECTION 652.
23. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY NECESSARY STREET/SIDEWALK OCCUPANCY OR OPENING PERMITS.
24. ALL SIGNAL HEADS AND SIGNS SHALL BE MOUNTED TO MAST ARMS WITH ASTROBRACKETS.
25. ALL CONFLICTING SIGNS AND PAVEMENT MARKINGS SHALL BE PERMANENTLY REMOVED FOLLOWING MAINE DOT SPECIFICATIONS.
26. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING CABINET PRINTS AS WELL AS RED-LINE AS-BUILT DRAWINGS OF THE FINAL WORK TO THE RESIDENT. THOSE DRAWINGS SHALL BE A CLEAN SET OF PLANS SHOWING ALL CHANGES, MODIFICATIONS, AND ELEVATIONS TO THE BID PLANS.
27. COUNTDOWN PEDESTRIAN HEADS SHALL HAVE AUDIBLE/TACTILE FEATURES AND SHALL BE INCIDENTAL TO ITEM 643.71. IF PUSH BUTTONS ARE LOCATED LESS THAN 10 FEET APART OR ON THE SAME POLE, EACH PUSH BUTTON SHALL PROVIDE THE FOLLOWING FEATURES: PUSH BUTTON LOCATOR TONE, TACTILE ARROW, SPEECH WALK MESSAGE FOR THE WALKING PERSON INDICATION, SPEECH PUSH BUTTON INFORMATION MESSAGE, IF FROM WHERE A PEDESTRIAN ENTERS THE CROSSWALK TO THE PEDESTRIAN HEAD ON THE OPPOSITE SIDE IS MORE THAN 100 FEET, THE NUMBERS SHALL BE AT LEAST 9 INCHES HIGH.
28. ALL PROPOSED PEDESTRIAN PUSH BUTTON LOCATIONS SHALL MEET THE AMERICANS WITH DISABILITIES ACT STANDARDS, LATEST EDITION. ALL SIGNAL EQUIPMENT INCLUDING CONTROLLER CABINET, UPRIGHTS, AND MAST ARMS SHALL BE GALVANIZED.
29. ALL SIGNAL EQUIPMENT INCLUDING UPRIGHTS AND MAST ARMS SHALL BE GALVANIZED.
30. ALL SIGNAL HEADS SHALL BE HEAVY DUTY POLYCARBONATE AND REINFORCED WITH METAL STIFFENERS.
31. PAYMENT FOR REMOVAL OF EXISTING SIGNS SHALL BE INCIDENTAL TO OTHER 645 ITEMS.
32. THE BOTTOM OF THE HOUSING OF NEW SIGNAL FACES SHALL BE AT LEAST 17 FEET BUT NOT MORE THAN 19 FEET ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY UNLESS OTHERWISE SPECIFIED.
33. THE CONTRACTOR IS DIRECTED TO STANDARD SPECIFICATION 718 FOR ADDITIONAL INFORMATION RELATED TO THE CONTROLLER AND CONTROLLER CABINET. STANDARD SPECIFICATION 718 EXPANDS UPON THE INFORMATION FOUND IN THESE NOTES. AS SUCH, THE MORE RESTRICTIVE LANGUAGE BETWEEN THESE NOTES AND STANDARD SPECIFICATION 718 SHALL GOVERN THE WORK TO BE PERFORMED UNDER THIS PROJECT.
34. THE SIGNAL SYSTEM MUST BE FULLY FUNCTIONAL AND FREE OF COMMUNICATIONS OR EQUIPMENT FAILURES FOR A PERIOD OF SEVEN (7) DAYS. IF PROBLEMS OCCUR, THEY SHALL BE RECTIFIED BY THE CONTRACTOR AND THE START UP PERIOD RESTARTED FOR ANOTHER SEVEN (7) DAYS.
35. UPON DEMONSTRATING A SUCCESSFUL 7 DAY START UP TEST, THE TOWN AND MAINE DOT SHALL EVALUATE THE OPERATION OF THE SYSTEM FOR A PERIOD OF 30 DAYS. SHOULD THE SYSTEM MALFUNCTION DURING THIS PERIOD THE CONTRACTOR SHALL MAKE ANY REPAIRS OR CORRECTIONS AND THE ACCEPTANCE TEST PERIOD WILL START OVER AGAIN. ACCEPTANCE TESTING MUST DEMONSTRATE TO THE TOWN AND MAINE DOT THAT ALL HARDWARE AND EQUIPMENT FUNCTION IN ACCORDANCE WITH THESE SPECIFICATIONS, REQUIREMENTS, THROUGH-PUTS AND FUNCTIONALITY.
36. THE CONTRACTOR SHALL WARRANT ALL WORK AND EQUIPMENT FOR A MINIMUM PERIOD OF ONE YEAR AFTER INSTALLATION AND ACCEPTANCE.
37. ALL REQUIRED OVERHEAD TRAFFIC SIGNAL WORK REQUIRED WITHIN ALL ROADWAY TRAVEL LANES SHALL BE COMPLETED BETWEEN 6:00 PM AND 6:00 AM UNLESS OTHERWISE APPROVED BY THE TOWN OF STANDISH. A CERTIFIED POLICE PRESENCE AND/OR CERTIFIED FLAGGER(S) IS MANDATORY FOR THE DURATION OF THE TRAVEL LANE CLOSURE(S).
38. STOP BAR VIDEO DETECTION (SBVD) SHALL BE SUPPLIED BY ONE OF THE FOLLOWING MANUFACTURERS:  
 A. GRIDSMART/CUBIC  
 B. MIO VISION  
 C. FLIR (THERMAL)  
 D. WAVETRONIX STOP BAR DETECTION SYSTEM
39. ADVANCED VEHICLE DETECTION (DILEMMA ZONE) SHALL BE THE WAVETRONIX SMART SENSOR ADVANCE, OR APPROVED EQUAL.
40. CONTRACTOR SHALL CONDUCT TEST PITS AS NOTED ON THE PLANS (AT A MINIMUM) OR AS NEEDED TO DETERMINE EXACT LOCATION OF EXISTING UNDERGROUND STRUCTURES. TEST PITS SHALL BE CONDUCTED AT LEAST 2 WEEKS PRIOR TO THE START OF CONSTRUCTION OR ORDERING OF MATERIALS.
41. SIGNAL INFRASTRUCTURE SHALL BE GALVANIZED.

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Rev.	Date	Revision

Issued For	Date	By

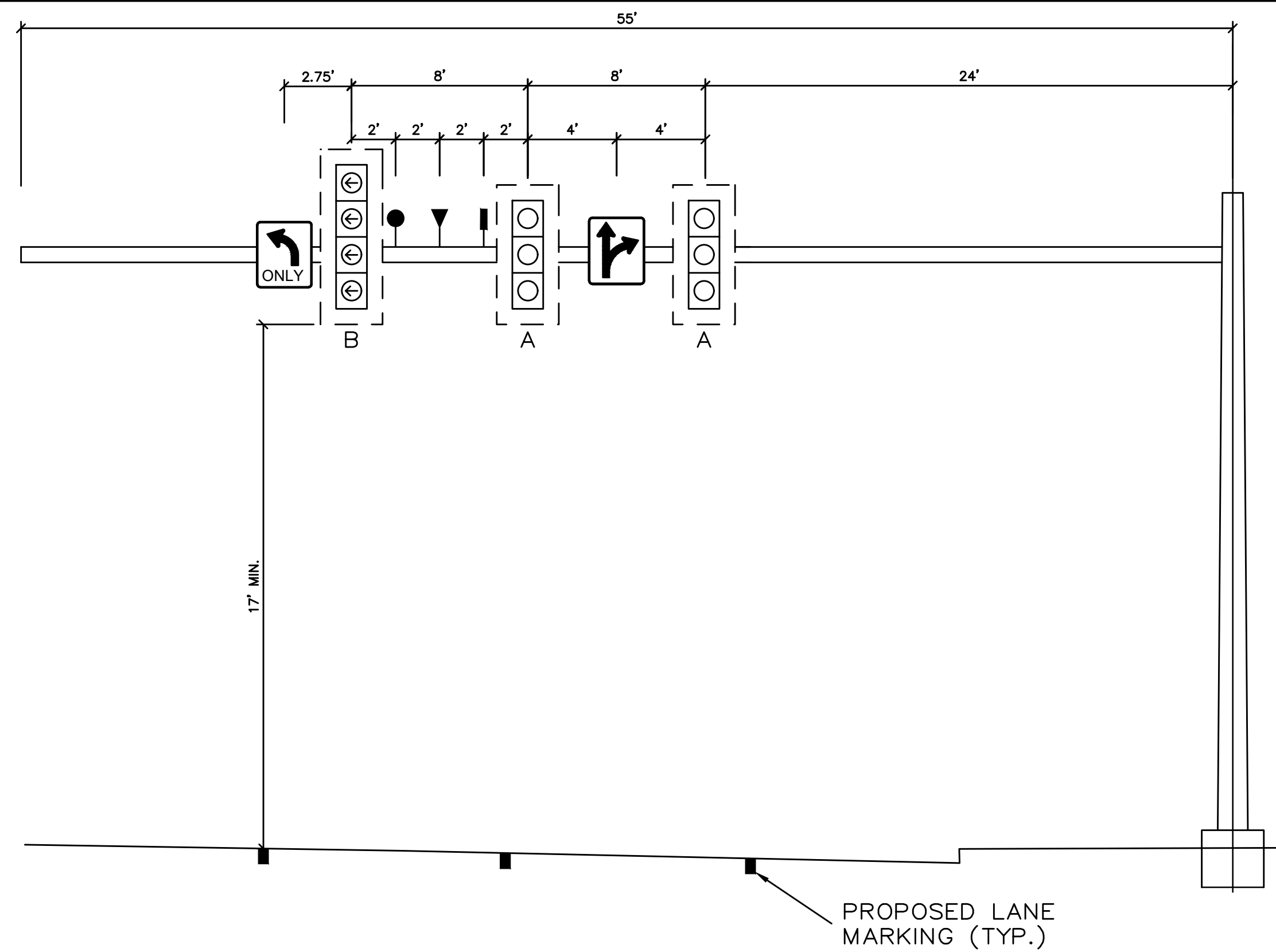
Design:	HWH	Draft:	LAN	Date:	OCT 2023
Checked:	WCH	Scale:	NTS	Job No.:	3863
File Name: 3863-SIGNAL.dwg					
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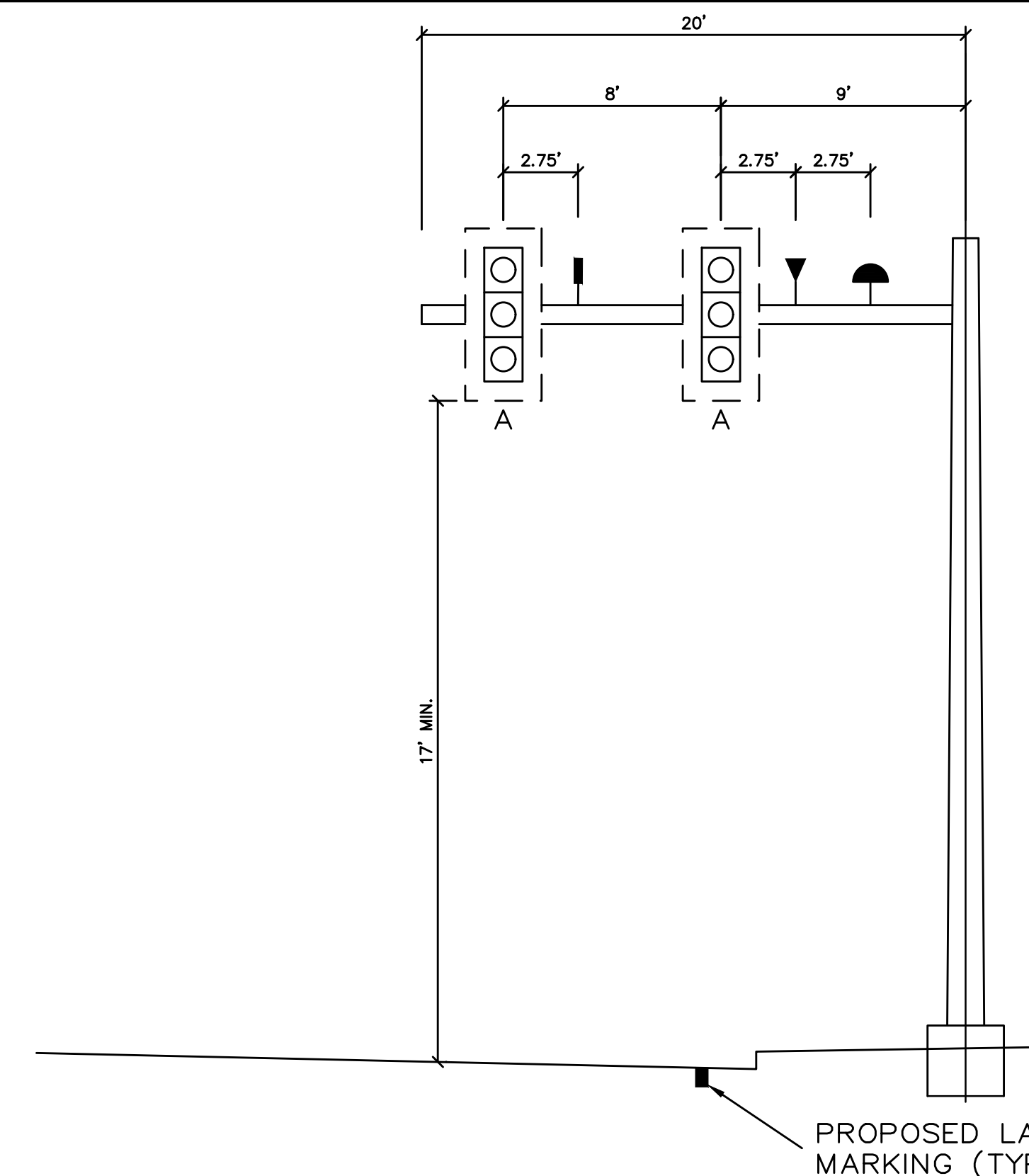
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Drawing Name:	<b>SIGNAL NOTES</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.	<b>17</b>
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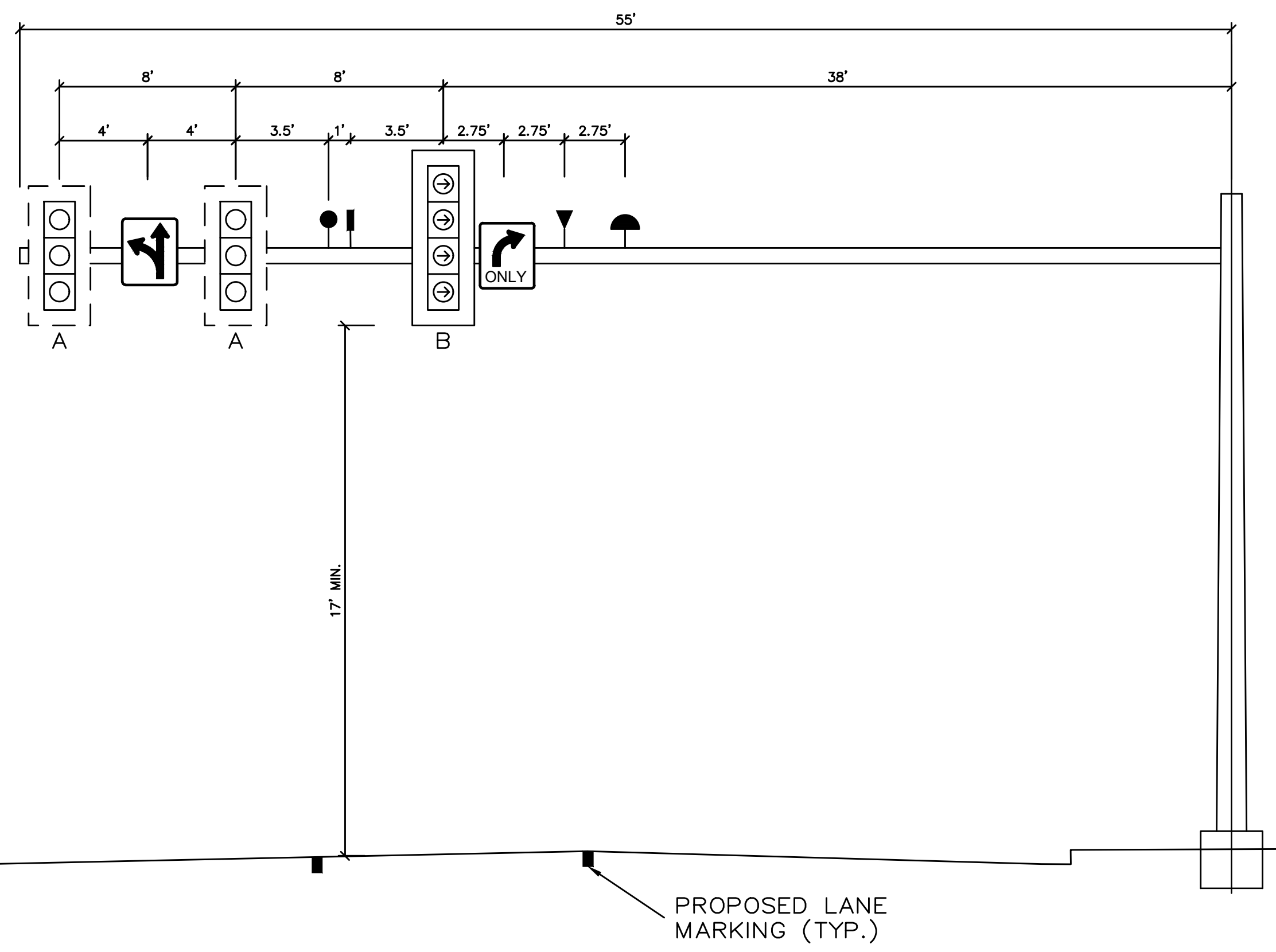
**SIGNAL FOR RICHVILLE ROAD SOUTHBOUND MAST ARM Q**  
NOT TO SCALE



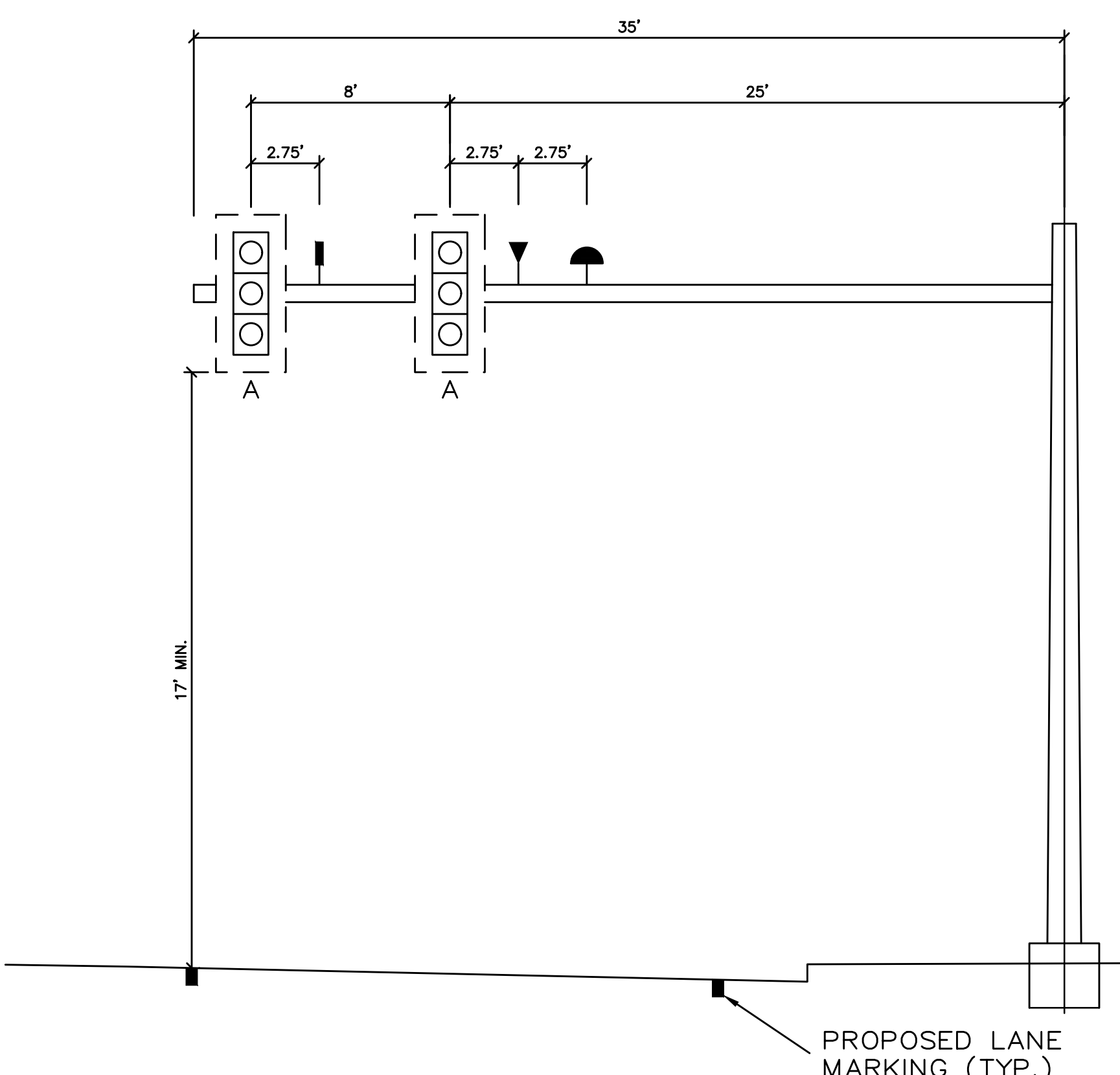
**SIGNAL FOR NORTHEAST ROAD NORTHBOUND MAST ARM G**  
NOT TO SCALE

LEGEND	
VEHICLE DETECTION	
DILEMMA ZONE DETECTION	
EMERGENCY PRE-EXEMPTION RECEIVER	
EMERGENCY PRE-EXEMPTION STROBE	

- NOTES:
- EQUIPMENT ATTACHED TO THE BASE OF DUAL PURPOSE POLES NOT SHOWN. OVERHEAD ILLUMINATION ALSO NOT SHOWN.
  - SEE SIGNAL PLANS FOR SIGNAL HEAD AND SIGNING INFORMATION.



**SIGNAL FOR FORT HILL ROAD NORTHBOUND MAST ARM Q**  
NOT TO SCALE



**SIGNAL FOR CHADBOURNE ROAD SOUTHBOUND MAST ARM Q**  
NOT TO SCALE

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Rev.	Date	Revision

Issued For	Date	By

Design:	HWH	Draft:	LAN	Date:	OCT 2023
Checked:	WCH	Scale:	NTS	Job No.:	3863
File Name: 3863-SIGNAL.dwg					
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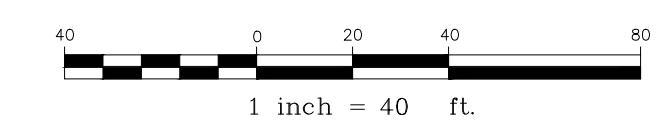
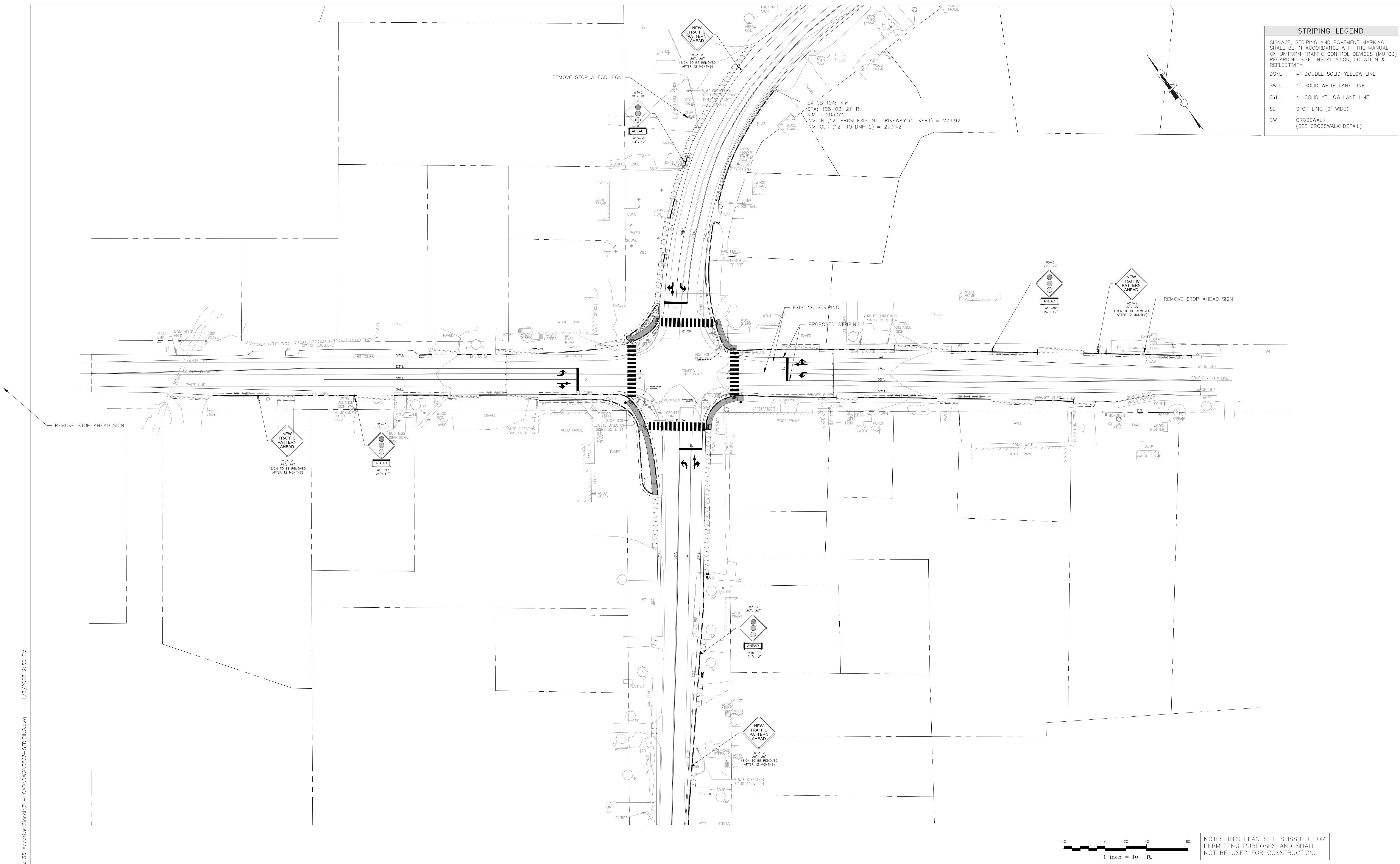
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Drawing Name:	<b>SIGNAL DETAILS</b>
Project:	<b>Layout &amp; Signalization of Route 114 &amp; 35 Intersection</b> Standish, Maine
Client:	<b>Town of Standish</b> 175 Northeast Road, Standish, Maine 04084

Drawing No.  
**18**



STRIPING LEGEND	
SIGNAGE, STRIPING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) REGARDING SIZE, INSTALLATION, LOCATION & REFLECTIVITY.	
DSYL	4" DOUBLE SOLID YELLOW LINE
SWLL	4" SOLID WHITE LANE LINE
SYLL	4" SOLID YELLOW LANE LINE
SL	STOP LINE (2' WIDE)
CW	CROSSWALK (SEE CROSSWALK DETAIL)



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Rev.	Date	Revision

Issued For	Date	By

Design: HWH | Draft: LAN | Date: OCT 2023  
 Checked: WCH | Scale: 1"=10' | Job No.: 3863  
 File Name: 3863-STRIPING.dwg  
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Drawing Name:	SIGNING & STRIPING PLAN
Project:	Layout & Signalization of Route 114 & 35 Intersection Standish, Maine
Client:	Town of Standish 175 Northeast Road, Standish, Maine 04084

Drawing No.  
 19